

CONTRACT NO. B-15745

INDEX					
PROJECT	STRUCTURE	TYPE	SPAN	OVER	STATION
FR-056-1(2)	154-77-3636A	RECONSTRUCTION AND OVERLAY	1 of 125'	TURMAN CREEK	195 + 37
SHEET NO.	SHEET DESCRIPTION	SUBJECT			SCALE
1	INDEX AND TITLE SHEET				
2	TRAFFIC MAINTENANCE				
3	GENERAL PLAN				
4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100	DETAILS				
6	SUPERSTRUCTURE				
7	EXPANSION JOINT CLASS 'C'S				
8	SUBGRADE				
9	EXPANSION JOINT CLASS 'C'S				

STATE OF INDIANA
INDIANA STATE HIGHWAY COMMISSION

BRIDGE PLANS FOR SPANS OVER 20 FEET ON STATE ROAD NO. 154 PROJECT NO. FR-056-1(2)

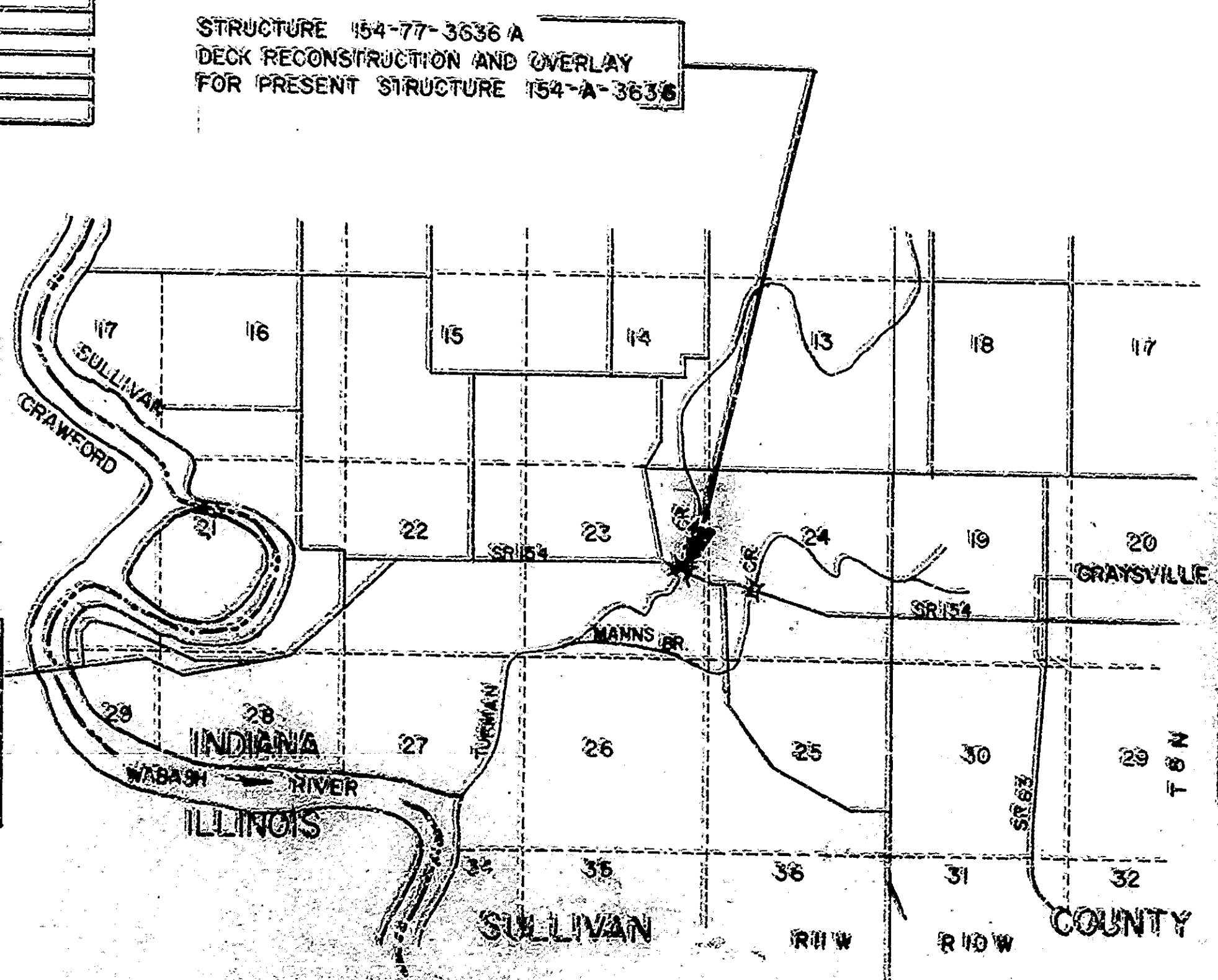
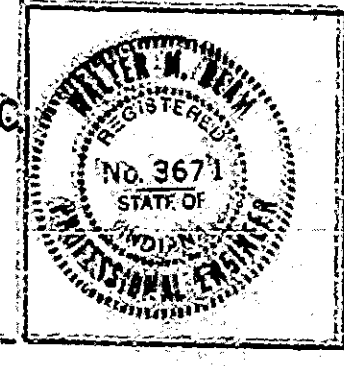
RECONSTRUCTION FOR BRIDGE ON SR 154 OVER TURMAN CREEK
2.2 MILES WEST OF SR 63
ALL IN SECTION 23, T8N, R11W, SULLIVAN COUNTY

INDEX CONTINUED			
SHEET NO.	SHEET DESCRIPTION	DATE	BY
1	INDEX AND TITLE SHEET		
2	TRAFFIC MAINTENANCE		
3	GENERAL PLAN		
4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100	DETAILS		
6	SUPERSTRUCTURE		
7	EXPANSION JOINT CLASS 'C'S		
8	SUBGRADE		
9	EXPANSION JOINT CLASS 'C'S		

TRAFFIC DATA	
A.A.T. (1975)	1500 V.P.D.
A.A.T. (1976)	V.P.D.
D.V.Y. (1976)	V.P.D.
TRUCKS	D.V.Y. & A.A.T.
DESIGN SPEED	V.P.D.
PAVE WIDTH	

NOTE: WHENEVER "INDIANA STATE HIGHWAY COMMISSION" APPEARS IN THESE PLANS, IT SHALL BE INTERPRETED AS "INDIANA DEPARTMENT OF HIGHWAYS" EXCEPT THE 1978 INDIANA STATE HIGHWAY COMMISSION SPECIFICATIONS SHALL BE USED.

THESE PLANS PREPARED BY
FLOYD E. BURROUGHS & ASSOCIATES, INC.
CONSULTING ENGINEERS
INDIANAPOLIS, INDIANA
CERTIFIED *Walter Adams* DATE *May 3, 1979*



STRUCTURE 154-77-3636A
DECK RECONSTRUCTION AND OVERLAY
FOR PRESENT STRUCTURE 154-A-3636

INDEX CONTINUED			
SHEET NO.	SHEET DESCRIPTION	DATE	BY
1	INDEX AND TITLE SHEET		
2	TRAFFIC MAINTENANCE		
3	GENERAL PLAN		
4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100	DETAILS		
6	SUPERSTRUCTURE		
7	EXPANSION JOINT CLASS 'C'S		
8	SUBGRADE		
9	EXPANSION JOINT CLASS 'C'S		

INDIANA STATE HIGHWAY COMMISSION
STANDARD SPECIFICATIONS DATED 1978
TO BE USED WITH THESE PLANS.

NO.	DATE	REVISION
1	5/3/79	ISSUED FOR BIDDING
2	5/3/79	REVISED PER COMMENTS
3	5/3/79	REVISED PER COMMENTS
4	5/3/79	REVISED PER COMMENTS
5	5/3/79	REVISED PER COMMENTS

APPROVED: *Walter Adams* 8-22-77
CHIEF HIGHWAY ENGINEER

INDIANA STATE HIGHWAY COMMISSION
DEPARTMENT OF TRANSPORTATION

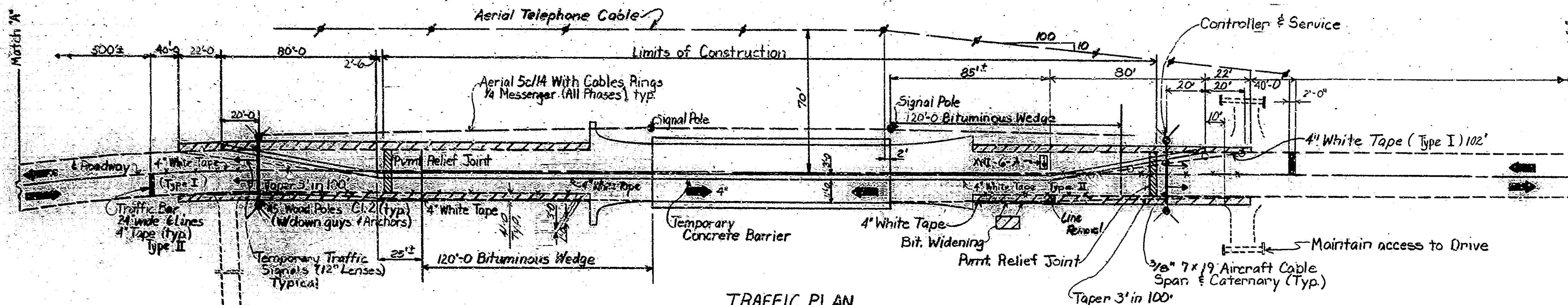
DESIGNED: *Walter Adams*
DRAWN: *Walter Adams*

PROJECT NO. 154-77-3636A

UTILITIES

General Telephone Company
8001 U.S. 24 West
Fort Wayne, Ind.

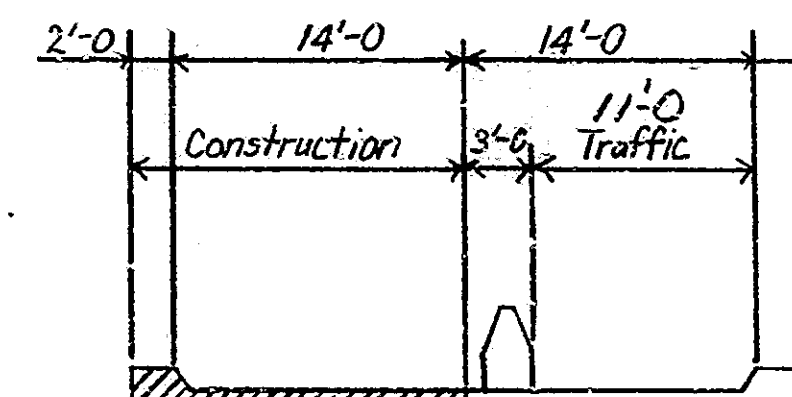
Note: Temporary wood poles to be located as far from the edge of pavement as possible within R/W.



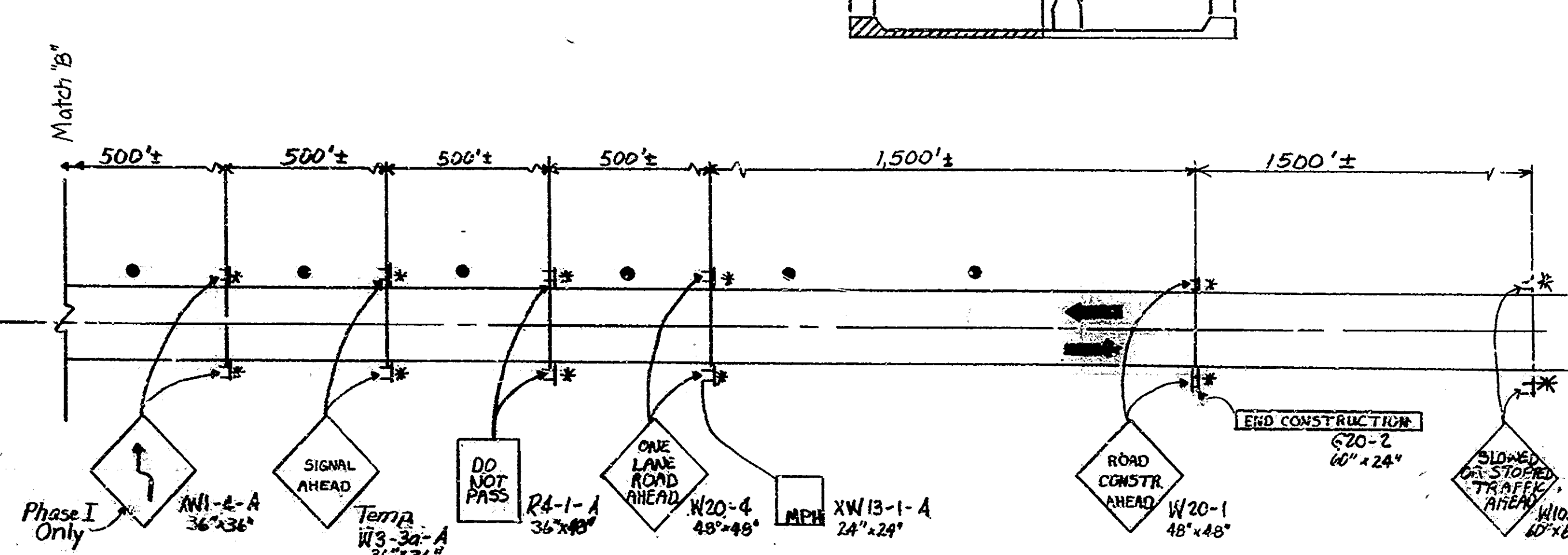
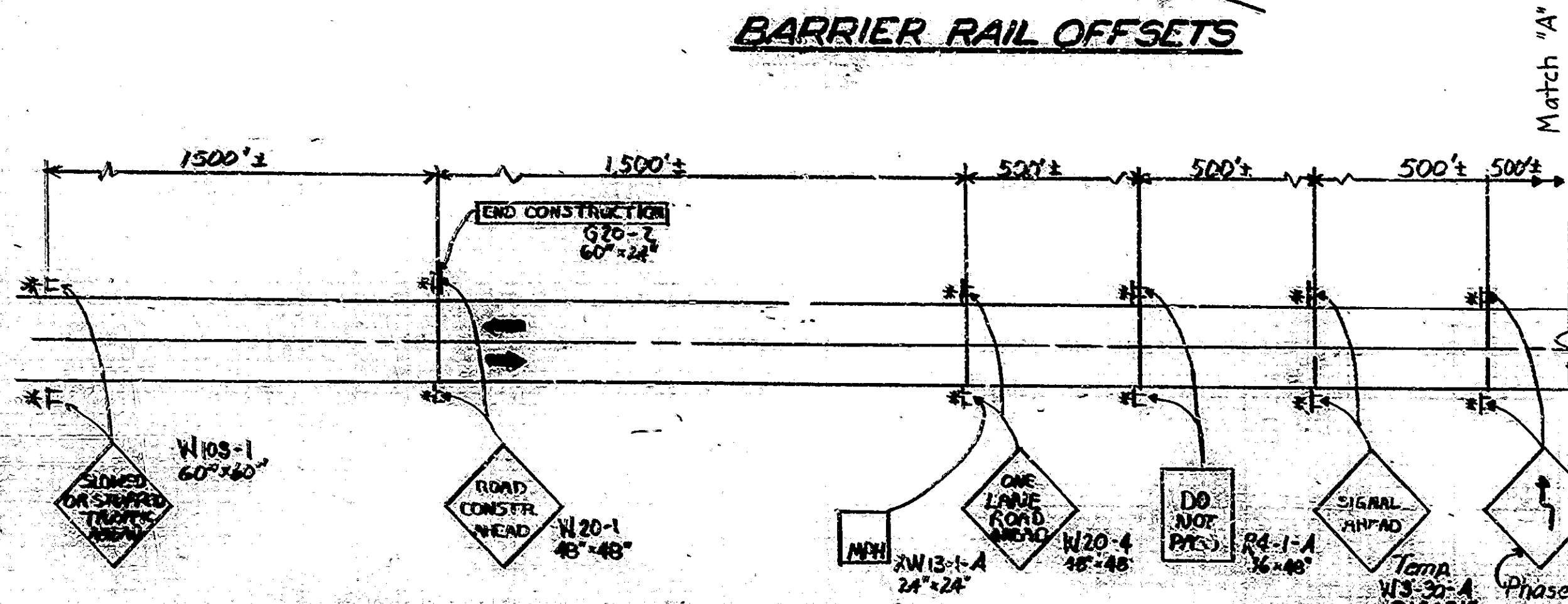
TRAFFIC PLAN

PHASE I Shown
PHASE II Reverse Temporary Concrete Barrier and Sign XW1-G-A

Contractor to furnish sufficient poles at 200' spacing to reach service point from controller. Location of controller may be changed if other service points are more accessible.



BARRIER RAIL OFFSETS



REMOVAL NOTES:

The existing solid white pavement edge lines adjacent to the bituminous widening and on the left side and bridge approach shall be removed from the E.B. Lane prior to setting the Barrier Rail to repair the N.B. Lane in Phase I.
The existing solid white pavement edge lines adjacent to the bituminous widening and on the left side of the bituminous wedge placed in Phase I shall be removed from the E.B. Lane prior to setting the Barrier Rail to repair of the E.B. Lane in Phase II.
The center line and yellow center line at both ends of the structure shall be removed from the Stop Bar and the Barrier shall be set on one lane traffic is established in the structure.

MAINTAINING TRAFFIC One Lump Sum
Includes the following:

Metal Drums or Barricades Type I or II
Cost of all equipment, materials, and labor (including poles, cables, 2" constant signal equipment and hardware) needed to install, operate and remove traffic signal system shall be included in the lump sum. Temp Traffic Signals

SEPARATE BAY ITEMS

- 21 EA Construction Signs Type A
- 4 EA Construction Signs Type B
- 4 EA Signal Heads 36" x 36" N.S. 30-A
- 520 LF Temporary Concrete Barrier
- 1650 LF Temporary Pavement Marking Type I
- 751 LF Temporary Lane Signposts
- 400 LF Temporary Lane Solid Yellow
- 1122 LF Lane Solid White
- 112 LF Lane Solid Yellow
- 400 LF Temporary Pavement Marking Type I
- 1 LSUM Temporary Traffic Signals

LEGEND

- * Low Intensity Flashing Yellow Light Type 'A'
- Bituminous Widening (Pave for as Bituminous Mixture for Approaches: 70MSY, HMC or H.A.E. Base 5D)
- ⊕ Signal Lights (12" Lenses)
- Signal Poles
- Metal Drums or Barricades Type I or II with Type C Steady Burning Lights (Not a Bay Item)

Note: Bituminous widening to remain in place.

NOTE: Bituminous Wedge and Pavement Relief Joint at east end of structure shall be placed with traffic controlled by flagmen.

Signal equipment may be either new or used. If signal equipment is used it must be in proper operating condition and meet the approval of the Engineer.
All signal equipment is to remain the property of the Contractor.

**TRAFFIC MAINTENANCE
INDIANA STATE HIGHWAY COMMISSION**

SCALE: 1" = 30'-0"

DATE: May 3, 1979

SUBMITTED FOR APPROVAL:

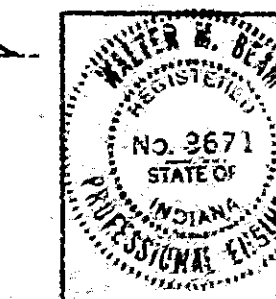
Halter

DRAWING OF SHEET 2 OF 26

PROJECT: FR-056-1(2)

CONTRACT NO. B-13745

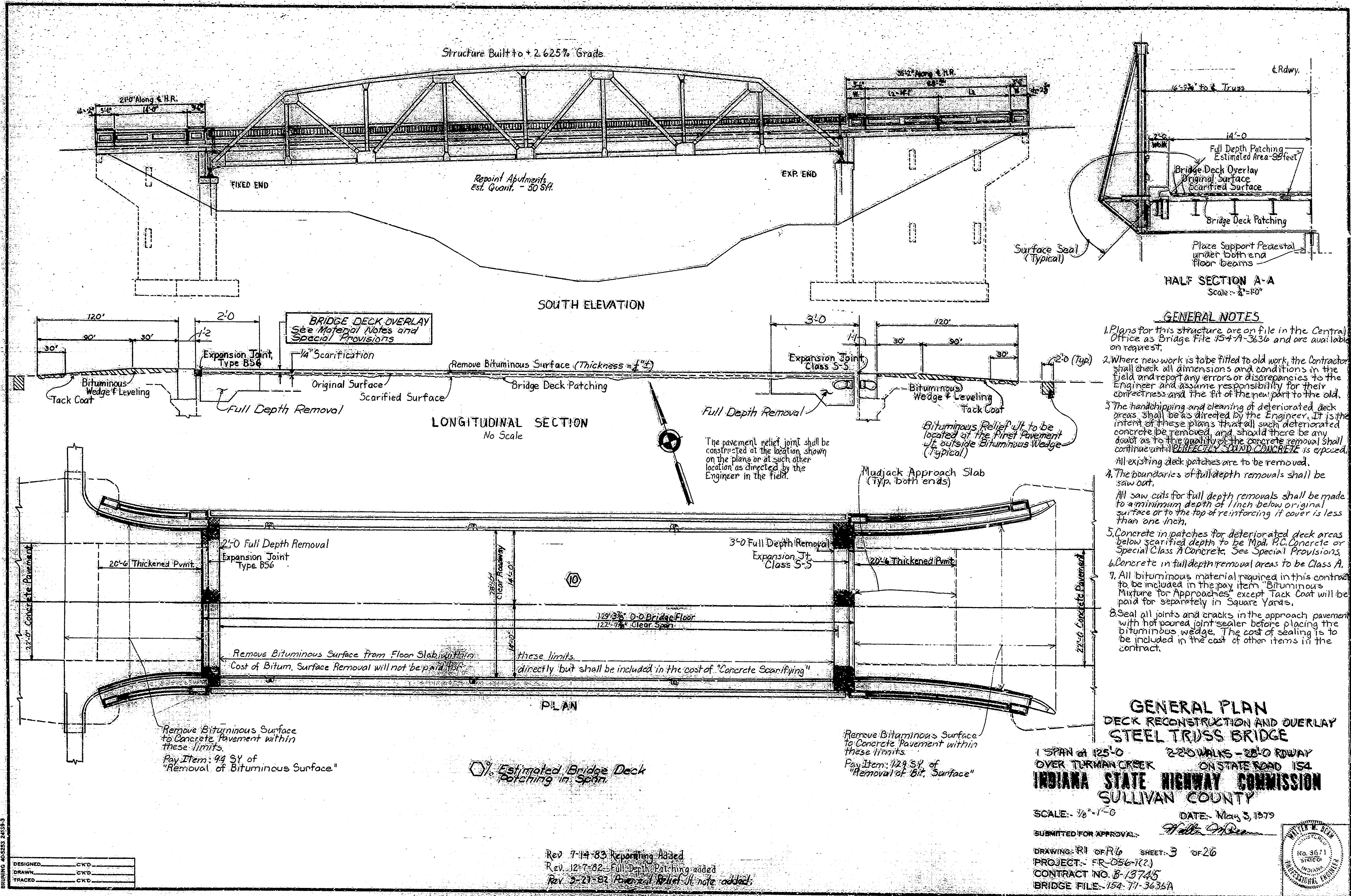
BRIDGE FILE: 154-77-363A



Rev. 7-15-83 Tape Designations
Rev. 12-20-82 Temp Traffic Signals added, signal equip note added
Rev. 12-7-82 Stop bars moved
Rev. 9-27-82 Sign designations, add special tape
Rev. 12-20-82 Aerial Telephone Cable Relocated

BUILDING 40-5855 2/1979

DESIGNED	CKD
DRAWN	CKD
TRACED	CKD



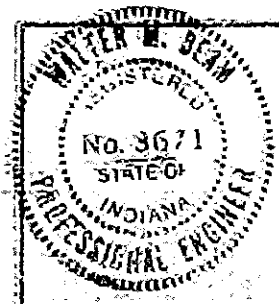
GENERAL NOTES

- Plans for this structure are on file in the Central Office as Bridge File 154-A-3636 and are available on request.
- Where new work is to be fitted to old work, the Contractor shall check all dimensions and conditions in the field and report any errors or discrepancies to the Engineer and assume responsibility for their correctness and the fit of the new part to the old.
- The handchipping and cleaning of deteriorated deck areas shall be as directed by the Engineer. It is the intent of these plans that all such deteriorated concrete be removed and should there be any doubt as to the quality of the concrete removal shall continue until PERFECTLY SOUND CONCRETE is exposed. All existing deck patches are to be removed.
- The boundaries of full depth removals shall be saw cut. All saw cuts for full depth removals shall be made to a minimum depth of 1 inch below original surface or to the top of reinforcing if cover is less than one inch.
- Concrete in patches for deteriorated deck areas below scarified depth to be Mod. P.C. Concrete or Special Class A Concrete. See Special Provisions.
- Concrete in full depth removal areas to be Class A.
- All bituminous material required in this contract to be included in the pay item "Bituminous Mixture for Approaches" except Tack Coat will be paid for separately in Square Yards.
- Seal all joints and cracks in the approach pavement with hot poured joint sealer before placing the bituminous wedge. The cost of sealing is to be included in the cost of other items in the contract.

GENERAL PLAN
DECK RECONSTRUCTION AND OVERLAY
STEEL TRUSS BRIDGE
 1 SPAN at 125'-0" 2-28" WALKS - 28'-0" RDWAY
 OVER TURMAN CREEK ON STATE ROAD 154
INDIANA STATE HIGHWAY COMMISSION
SULLIVAN COUNTY

SCALE: 1/8" = 1'-0" DATE: May 3, 1979

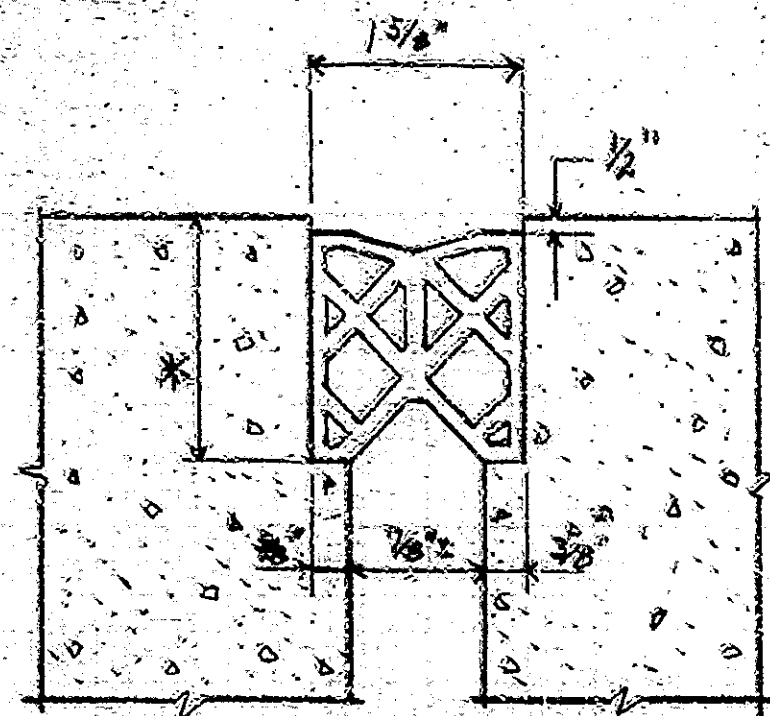
SUBMITTED FOR APPROVAL: *H. L. ...*
 DRAWING: R1 OF R16 SHEET: 3 OF 26
 PROJECT: FR-056-1(2)
 CONTRACT NO. B-13745
 BRIDGE FILE: 154-71-3636A



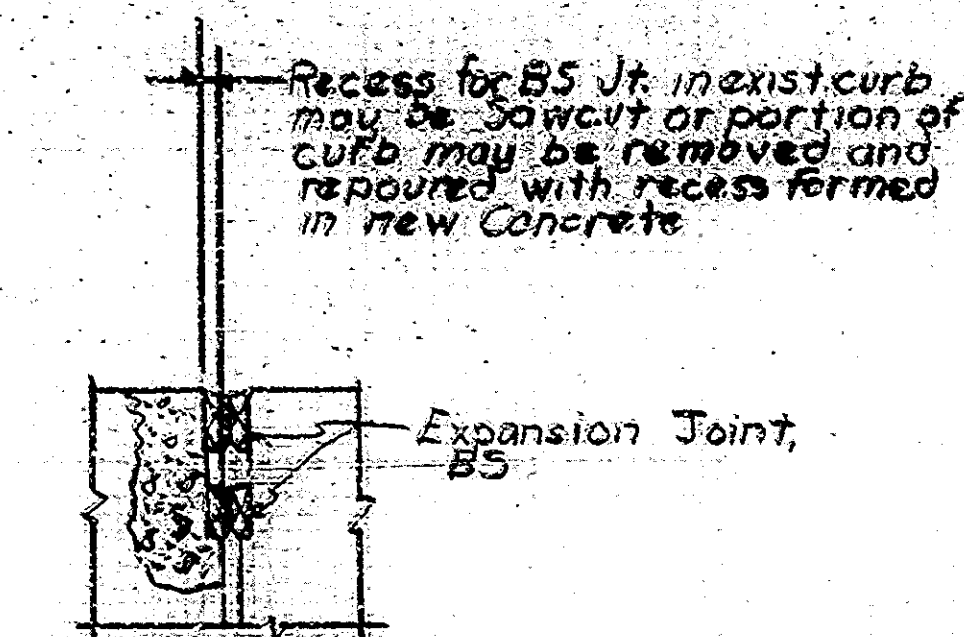
Rev 7-14-83 Repointing Added
 Rev 12-7-82 Full Depth Patching added
 Rev 5-21-82 Pavement Relief Jt. note added.

DESIGNED	CYD
DRAWN	CYD
TRACKED	CYD

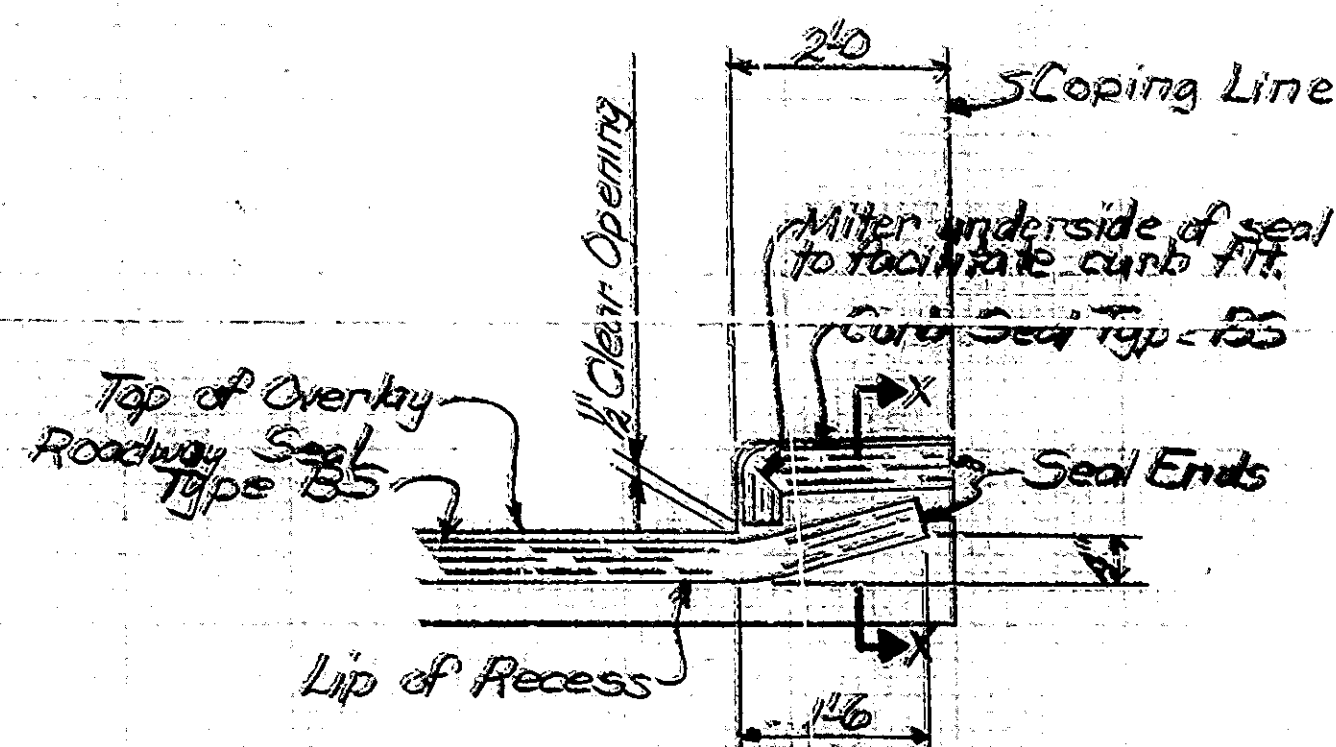
BRUNING 40-5283 24159-3



EXP. JOINT TYPE BS
* To be determined in the field. See the Special Provisions.

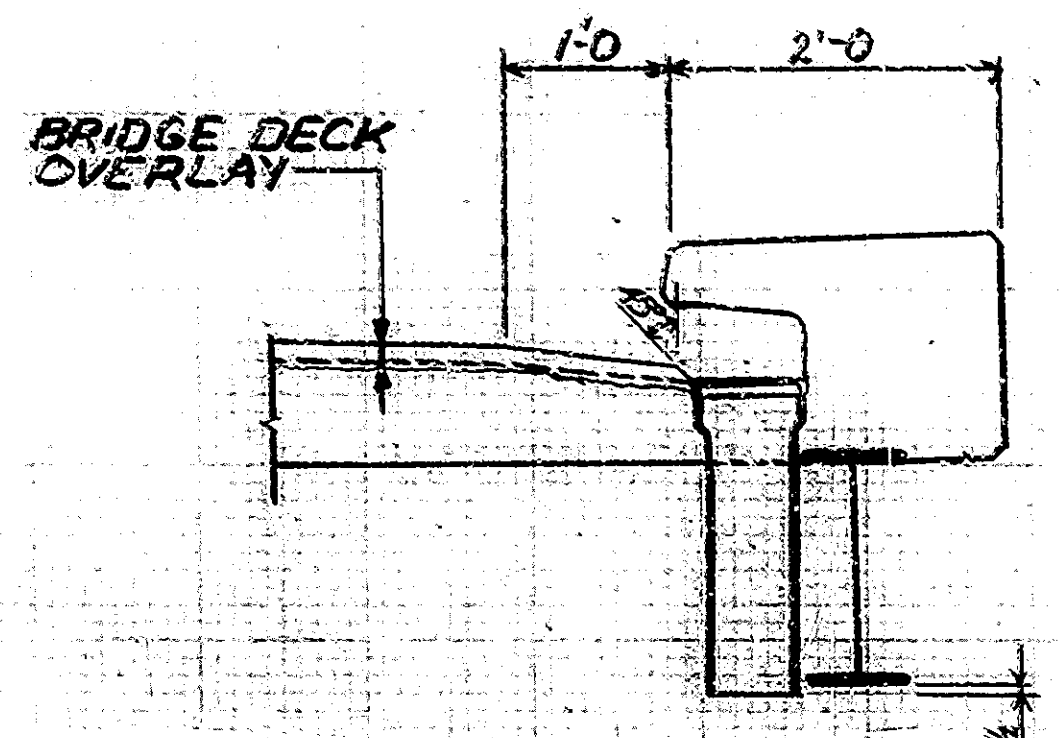


SECTION X-X No Scale



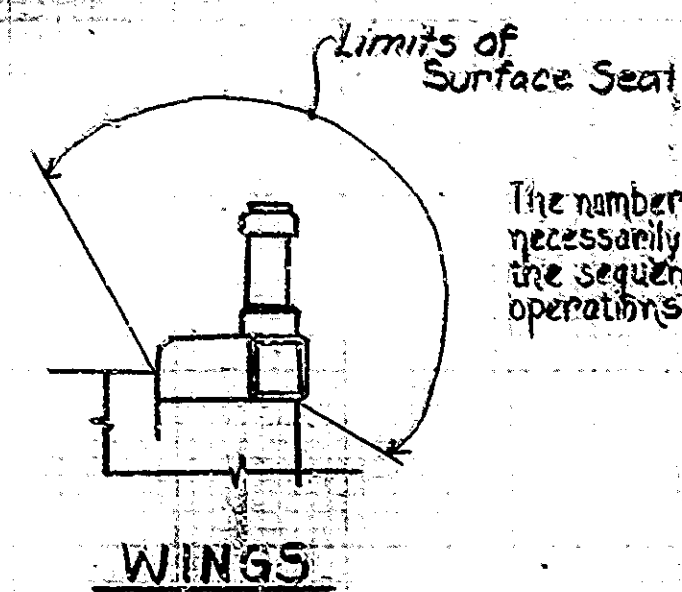
TYPICAL BS JOINT INSTALLATION AT CURBS No Scale

Clean out and rebuild existing curb and walk joints to accommodate joint seal such work to be included in cost of Expansion Joint Type BS.



OVERLAY AT ROADWAY DRAIN Scale 1"=4'-0"

STANDARD DRAWINGS		Purpose
Br-Std.	Rd-Std.	
CB		Reinf. Bar Notes
CB3		Type A Constr. Jt. Notch at Ends of Steel Beams, Jt. Details
Sht GR 6		Post Paddle Detail
Sht GR 5		Alum. Guard Rail Details
Sht GR 8		Steel Beam Guard Rail Class D5
Sht GR 10		Guard Rail Buried Ends
Sht GR 9		Alum. Beam Guard Rail Class DA
CB 2		Temp. Conc. Barrier
Sht 1		Std. Detour Signs
Sht 2A		Std. Detour Signs
Sht 3A		Std. Detour Signs
Sht 4		Std. Detour Signs
Sht 5		Std. Detour Signs
Sht 9		Traffic Sign Details
Sht GR 7		Class BS Guard Rail
Sht GR 9		Class BA Guard Rail



The numbers do not necessarily indicate the sequence of operations.

TRAFFIC PROCEDURE

1. Construct Bituminous Widening

PHASE I

A. MAINTAIN TRAFFIC on South Half of Bridge
B. CONSTRUCTION: Complete Construction on North Half of Bridge and Approaches.

PHASE II

A. MAINTAIN TRAFFIC on North Half of Bridge
B. CONSTRUCTION: Complete Construction on South Half of Bridge and Approaches.

CONSTRUCTION PROCEDURE

1. Remove the floor slab full depth as shown on details.
2. Scarify the remaining bridge floor to a depth of 1/4 inch and remove bituminous surface. Scarify additional areas of the bridge floor an additional 1/4 inch as directed by the Engineer.
3. Remove scarified dust.
4. Remove all existing deck patches and all deteriorated concrete below the level of scarification and remove concrete around reinforcing and along curbs, inaccessible to scarifying equipment, by hand chipping and cleaning in accordance with the Special Provisions.
5. Repour the full depth slab removal areas to level of scarification as shown on the plans. Class S-S Expan. Joint extensions to be present in concrete at Abut. No. 2.
6. Blast and clean all required deck areas and all removal and scarified areas.
7. Place the Bridge Deck Patching and Bridge Deck Overlay as shown on the Plans and in accordance with the Special Provisions, Install Expansion Joints.
8. Clean and Seal curbs, walks and coping from gutter to outside beam as shown on plans. Also include all surfaces of concrete railing and curbs at abutments, and tops of medians.
9. Construct pavement relief joints, bituminous wedges, and all other work shown on the plans, including the removal and installation of the guard rail.
10. All removal equipment used for partial concrete removals of bridge structures shall be hand held. Pneumatic hammers, 30 lbs. maximum weight shall be used for all removal areas to be patched and all areas within 24 inches of full depth removal lines. Pneumatic hammers, up to 90 lbs. maximum weight may be used for all other removals outside these limits. Deck areas that are to be removed full depth shall be completely separated from adjacent concrete before hammers heavier than 30 lbs. may be used.

MATERIAL NOTES

- Bridge Deck Overlay to be:
 - 1 1/2" Modified Portland Cement Concrete Overlay or
 - 2 1/2" Dense Portland Cement Concrete Overlay.
 - (See Special Provisions).
- BITUMINOUS WEDGE & BITUMINOUS LEVELING**
 - 110#/SY Bituminous Surface (Type 11B)
 - over
 - Variable Depth Bituminous Binder.
- BITUMINOUS RELIEF JOINT**
 - 110#/SY Bituminous Surface (Type 11B)
 - over
 - 1870#/SY Bituminous Base
- BITUMINOUS WIDENING**
 - 990#/SY Bituminous Base, Type 5D
- The maximum depth of Bituminous Surface Type 11B shall not exceed 1 1/2". At all locations where total wedge thickness will exceed 1 1/2", bituminous binder shall be placed as a first course to within one inch of the finished grade.

DETAILS

INDIANA STATE HIGHWAY COMMISSION

SCALE: - As Noted DATE: - May 3, 1979

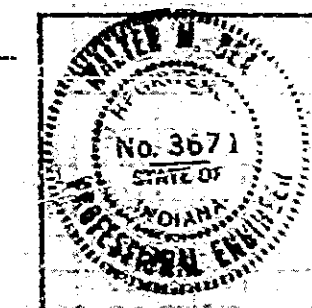
SUBMITTED FOR APPROVAL: *Walter G. Ben*

DRAWING: R2 OF R6 SHEET: 4 OF 26

PROJECT: FR-056-1(2)

CONTRACT NO. B-13745

BRIDGE FILE: 154-77-3636A

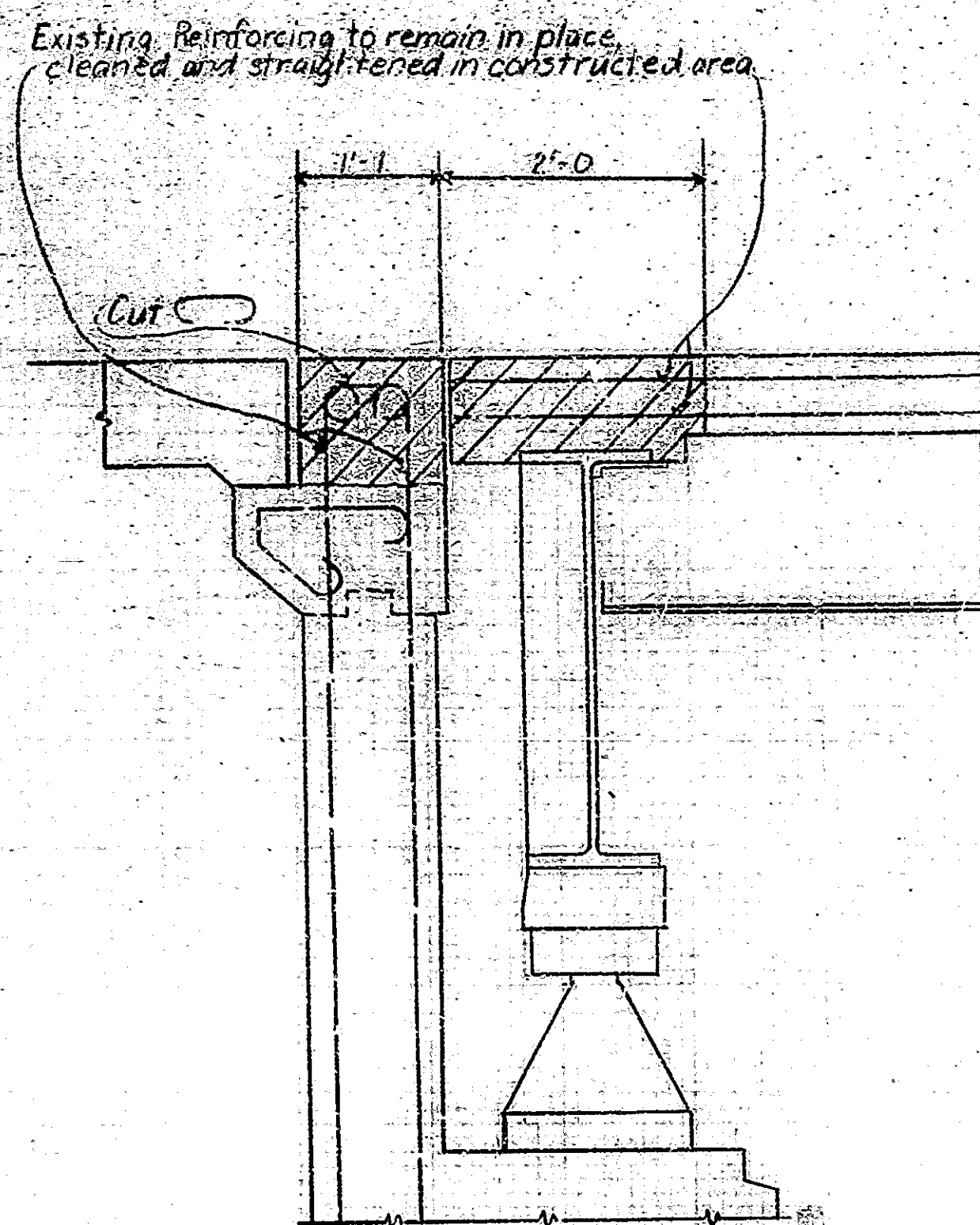


Bitum. Surface Removal Limits on Approaches (See Drawing R1)

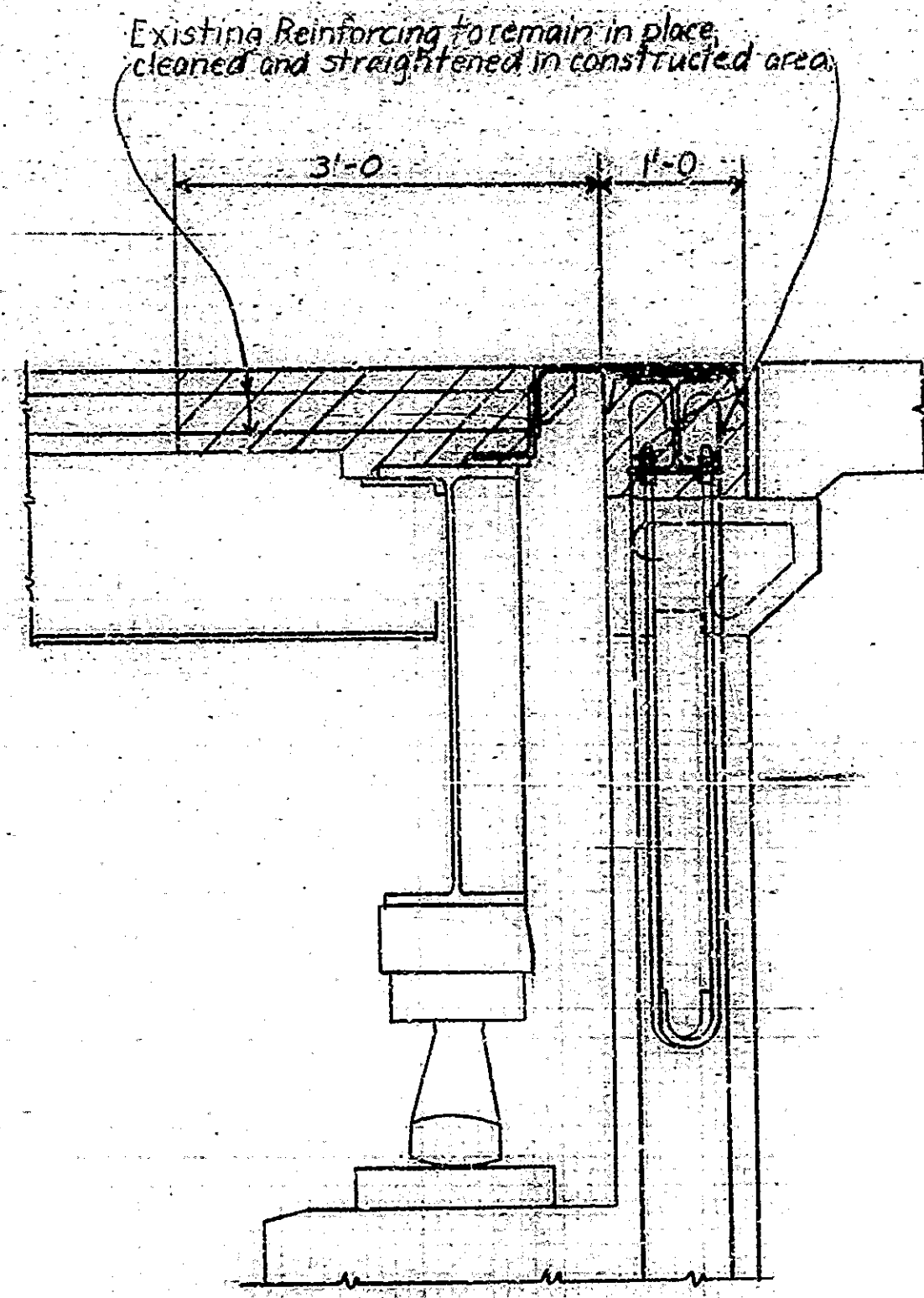
BITUMINOUS WEDGE AND LEVELING DETAIL No Scale

- Wedge to be a continuation of Bridge Deck Profile.
- Taper wedge uniformly to meet existing roadway surface Tack Coat to be paid for by Bituminous Material for Tack Coat in Sq. Yds.
- 1" 2" 1" Dense overlay, removal to added, 4" deep.

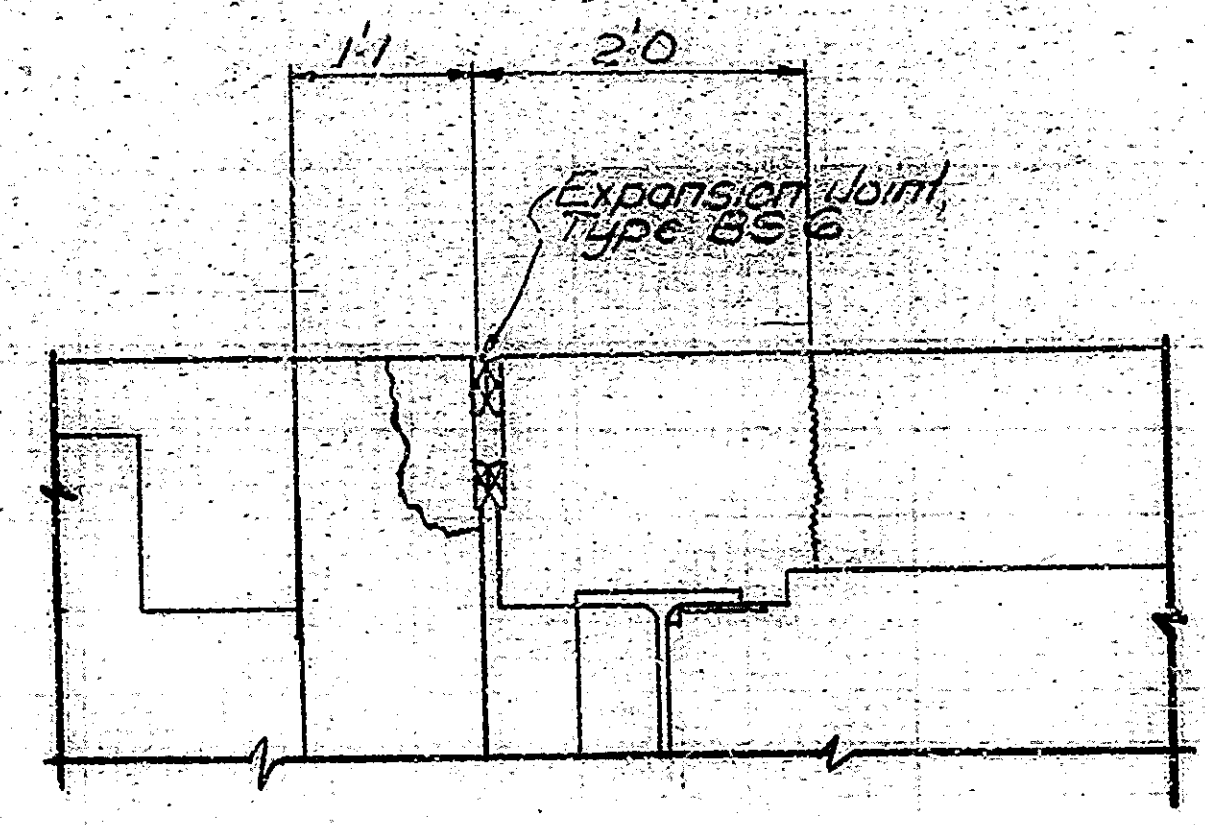
DESIGNED	BY
DRAWN	BY
TRACED	BY



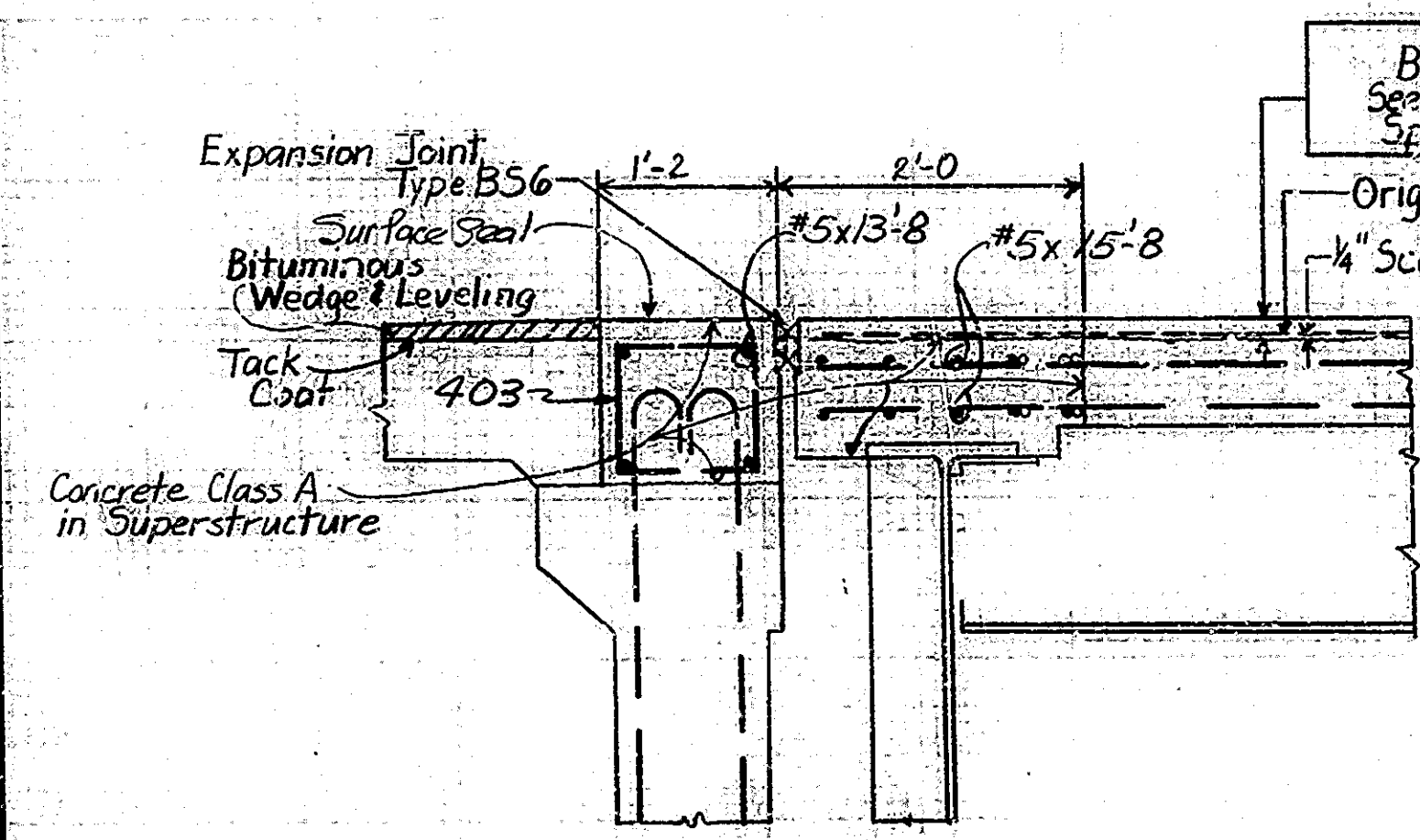
ABUTMENT 1 REMOVAL



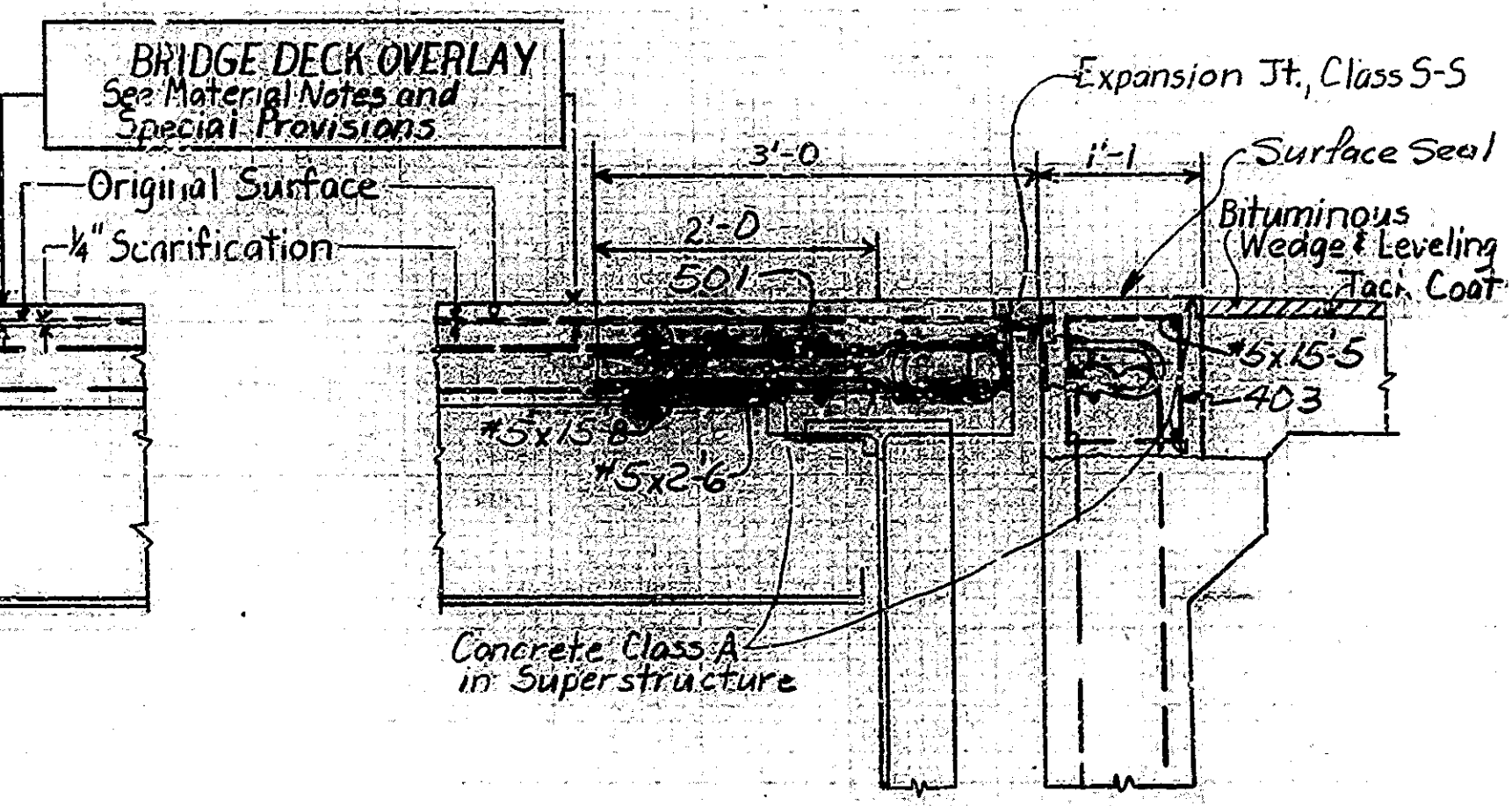
ABUTMENT 2 REMOVAL



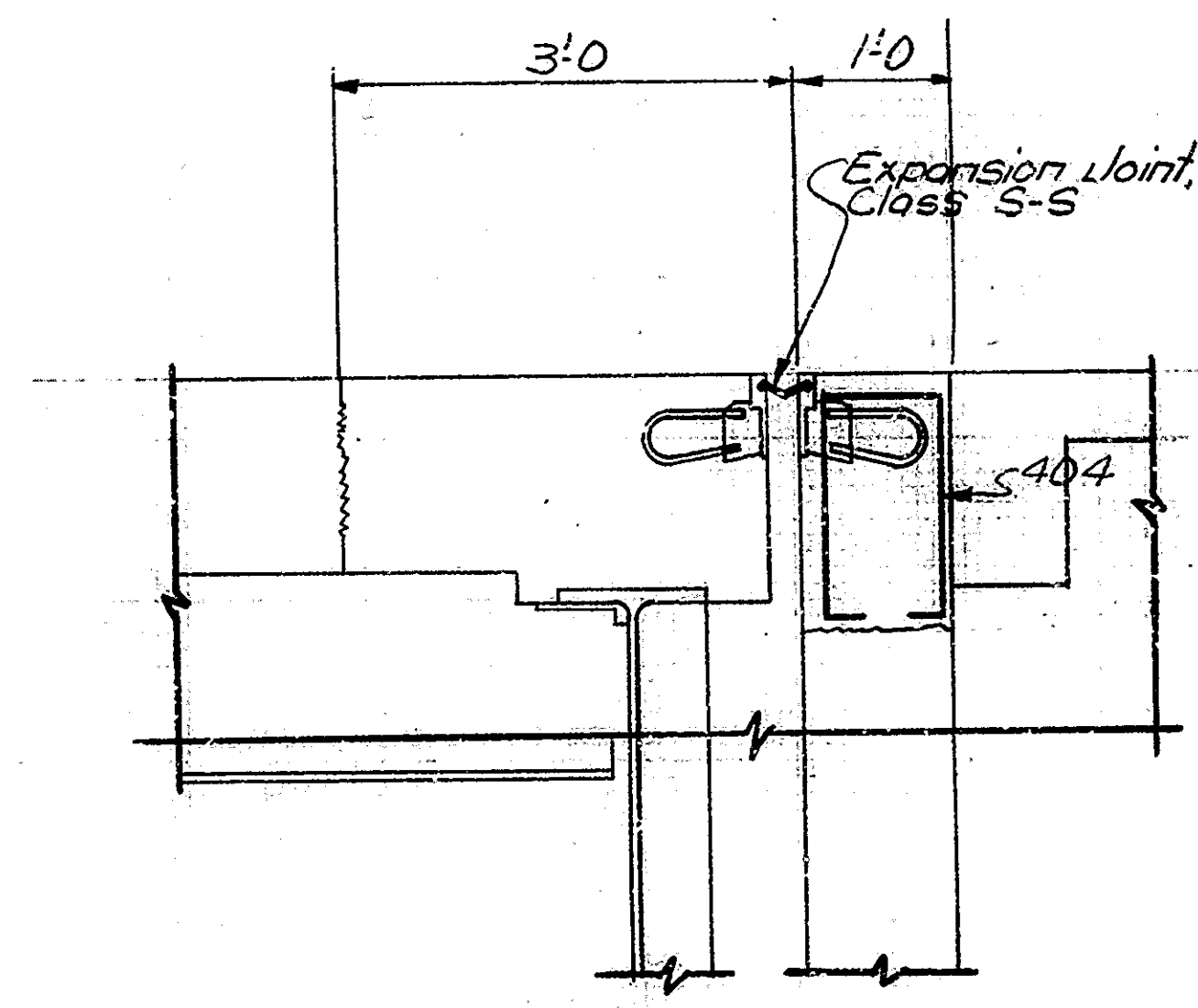
ABUTMENT No. 1 CURB SECTION



ABUTMENT 1



ABUTMENT 2



ABUTMENT No. 2 CURB SECTION

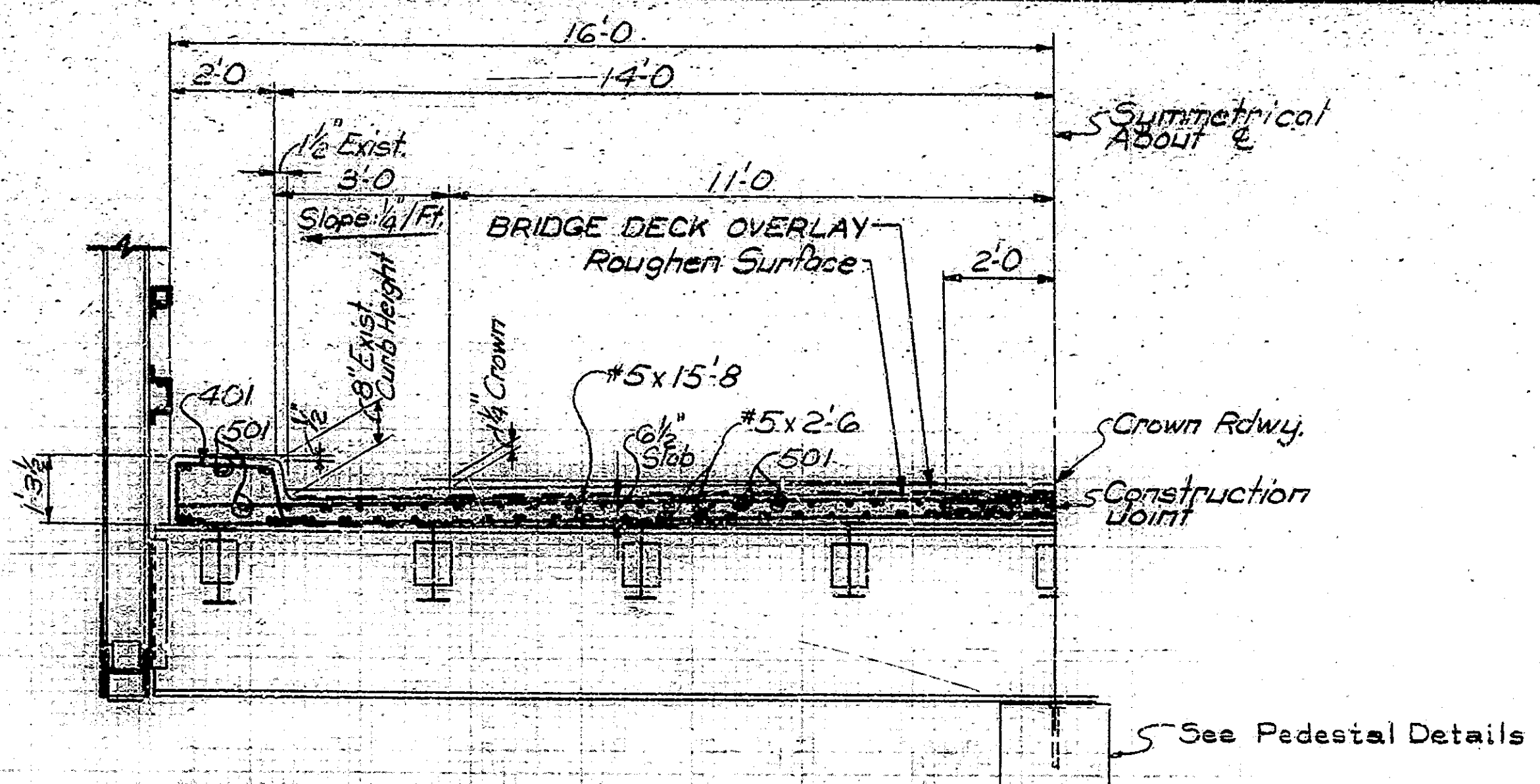
DETAILS
INDIANA STATE HIGHWAY COMMISSION

SCALE: 1" = 1'-0"
DATE: May 3, 1919
SUBMITTED FOR APPROVAL: *Halla M. Rose*
DRAWING: R3 OF R6 SHEET: 5 OF 26
PROJECT: FR-056-1(2)
CONTRACT NO. B-13745
BRIDGE FILE: 15A-77-3636A



DESIGNED	CK'D
DRAWN	CK'D
TRACED	CK'D

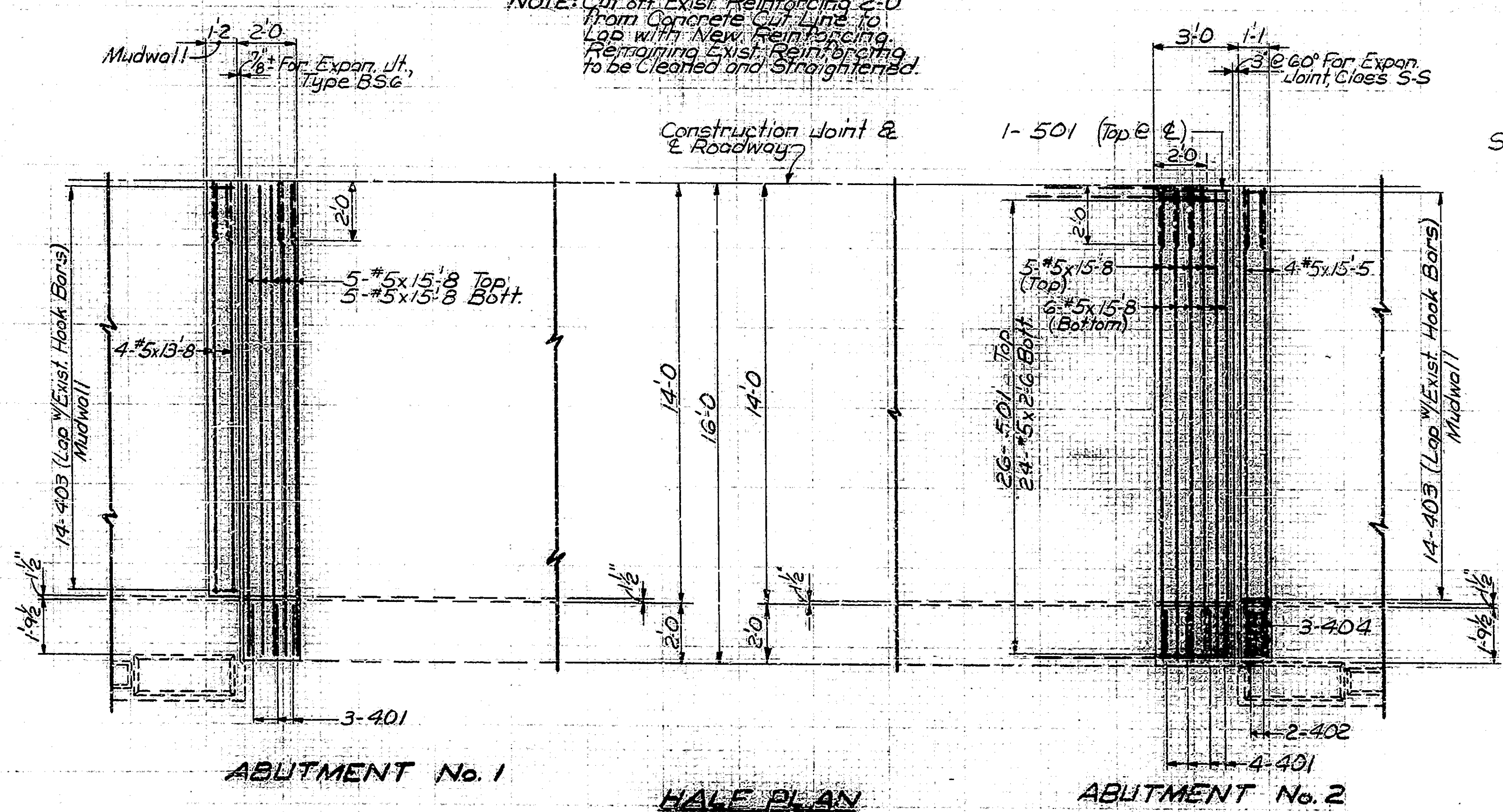
BRUNING 403283 24139-5



TYPICAL SECTION AT ABUTMENT #2 Scale: 1/2" = 1'-0"

ABUTMENT #1 SAME EXCEPT NO NEW LONGITUDINAL STEEL (Existing Longitudinal Steel to Remain in Place and to be Cleaned and Straightened.)

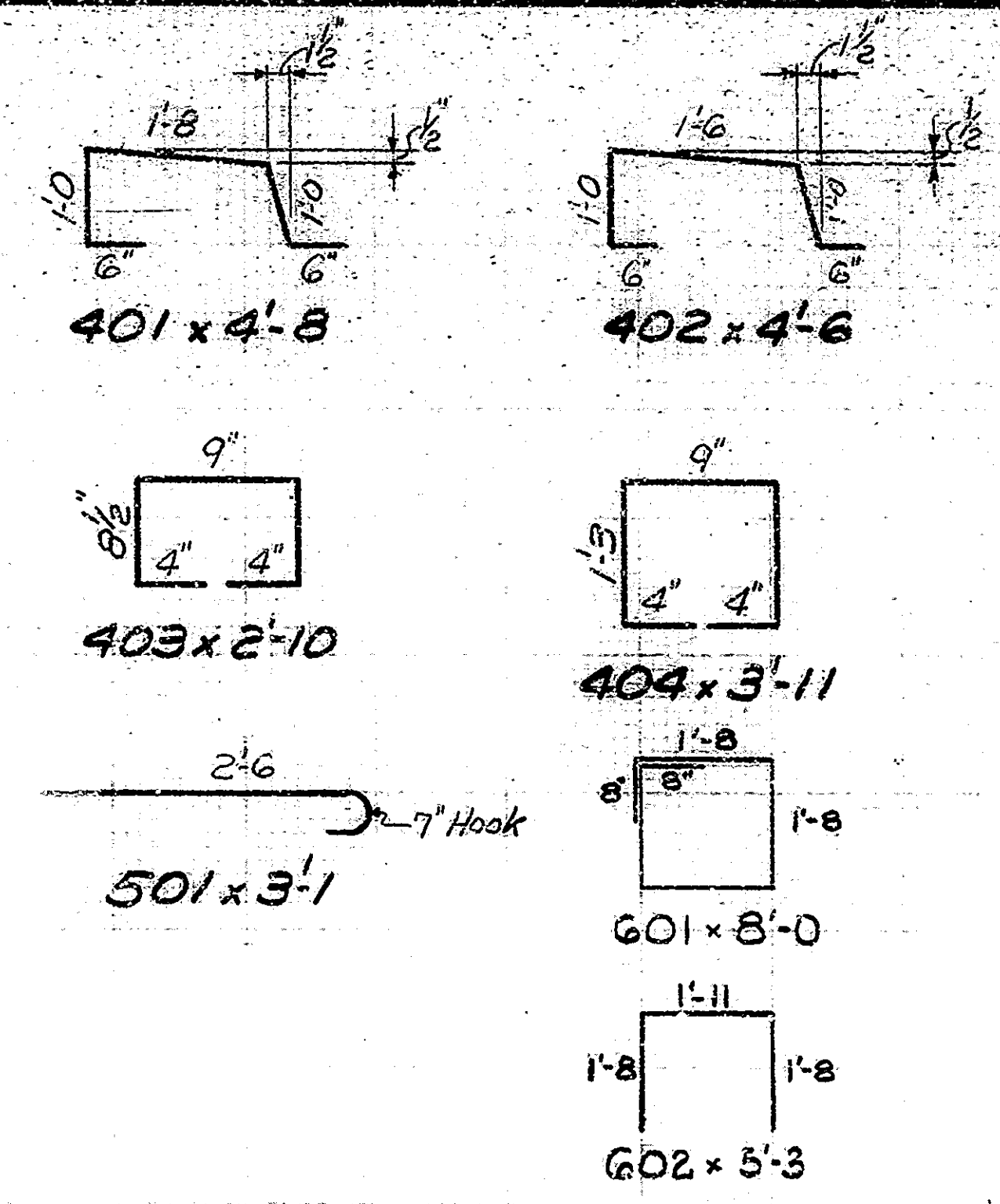
NOTE: Cut off Exist. Reinforcing 2'-0" from Concrete Cut Line to Lap with New Reinforcing. Retaining Exist. Reinforcing to be Cleaned and Straightened.



ABUTMENT No. 1

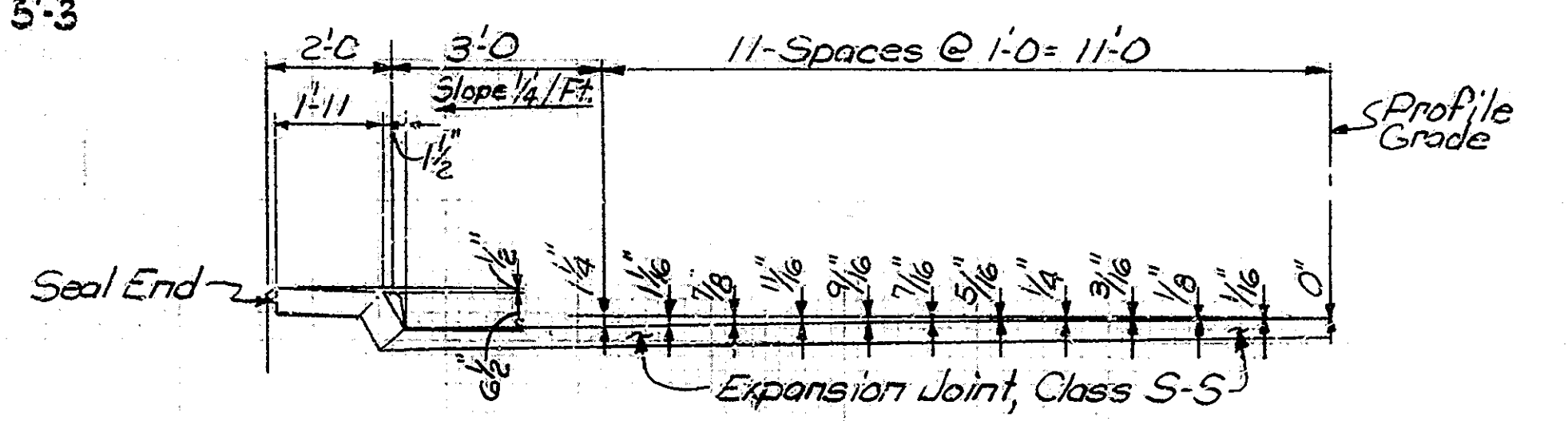
HALF PLAN Scale: 3/8" = 1'-0"

ABUTMENT No. 2

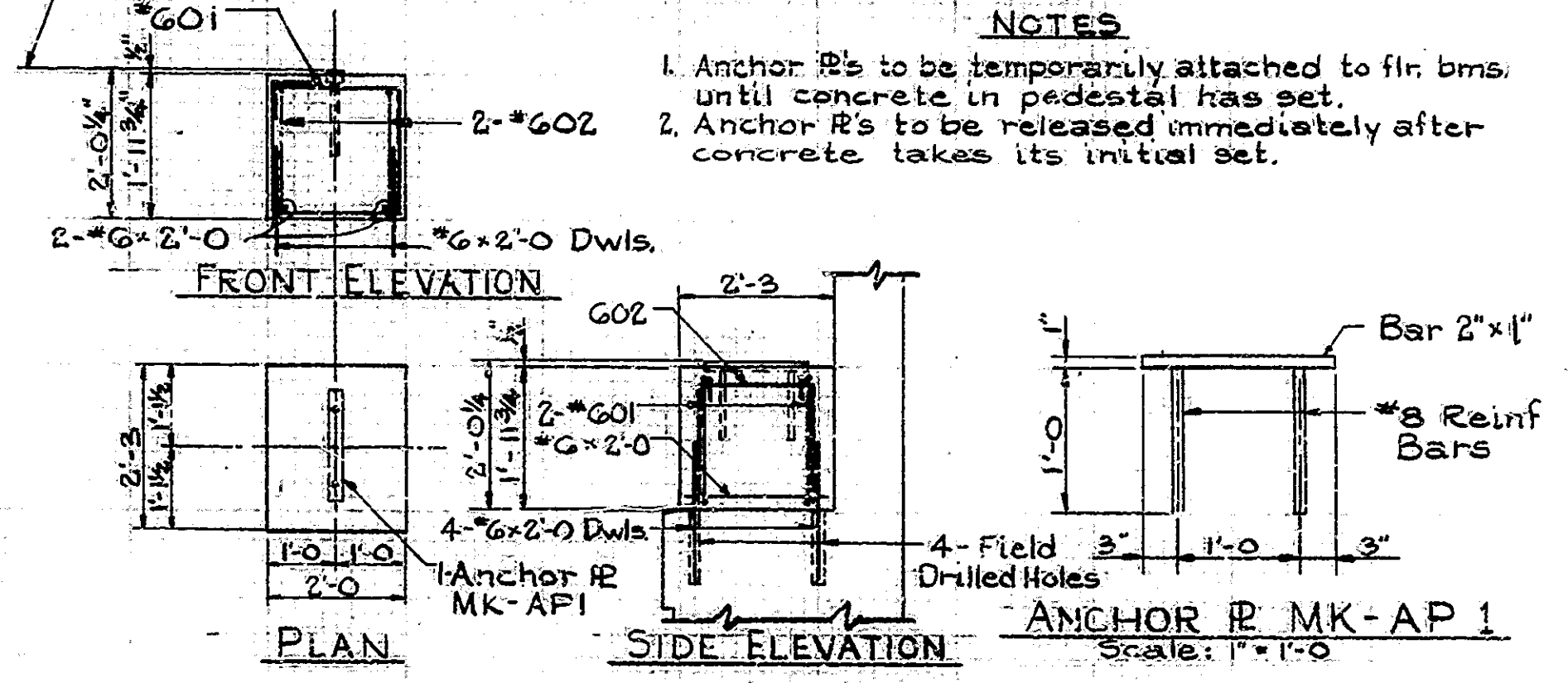


REINFORCING STEEL			
Size or Mark of Bars	Number	Length (FT.)	Weight (Lbs.)
#5	20	15'-8"	
#5	3	13'-8"	
Total No. 5			441
401	6	4'-8"	
403	28	2'-10"	
Total No. 9			72
601	2	8'-0"	
602	2	5'-3"	
#6	6	2'-0"	
Total No. 6			58
TOTAL REINF. STEEL			571
CONCRETE			
Conc. Class 'A' in Supstr.			3.1 CY
MISCELLANEOUS			
Anchor R. MK-API			1 Ea.
Field Drilled Holes in Conc.			4 Ea.

REINFORCING STEEL			
Size or Mark of Bars	Number	Length (FT.)	Weight (Lbs.)
501	53	3'-1"	
#5	22	15'-8"	
#5	8	15'-5"	
#5	48	2'-6"	
Total No. 5			784
401	8	4'-8"	
402	4	4'-6"	
403	28	2'-10"	
404	6	3'-11"	
Total No. 4			106
601	2	8'-0"	
602	2	5'-3"	
#6	6	2'-0"	
Total No. 6			58
TOTAL REINF. STEEL			948
CONCRETE			
Conc. Class 'A' in Supstr.			3.9 CY
MISCELLANEOUS			
Anchor R. MK-API			1 Ea.
Field Drilled Holes in Conc.			4 Ea.



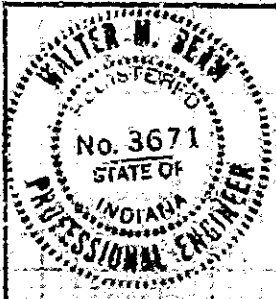
S-S JOINT INSTALLATION AND PAVEMENT OFFSETS



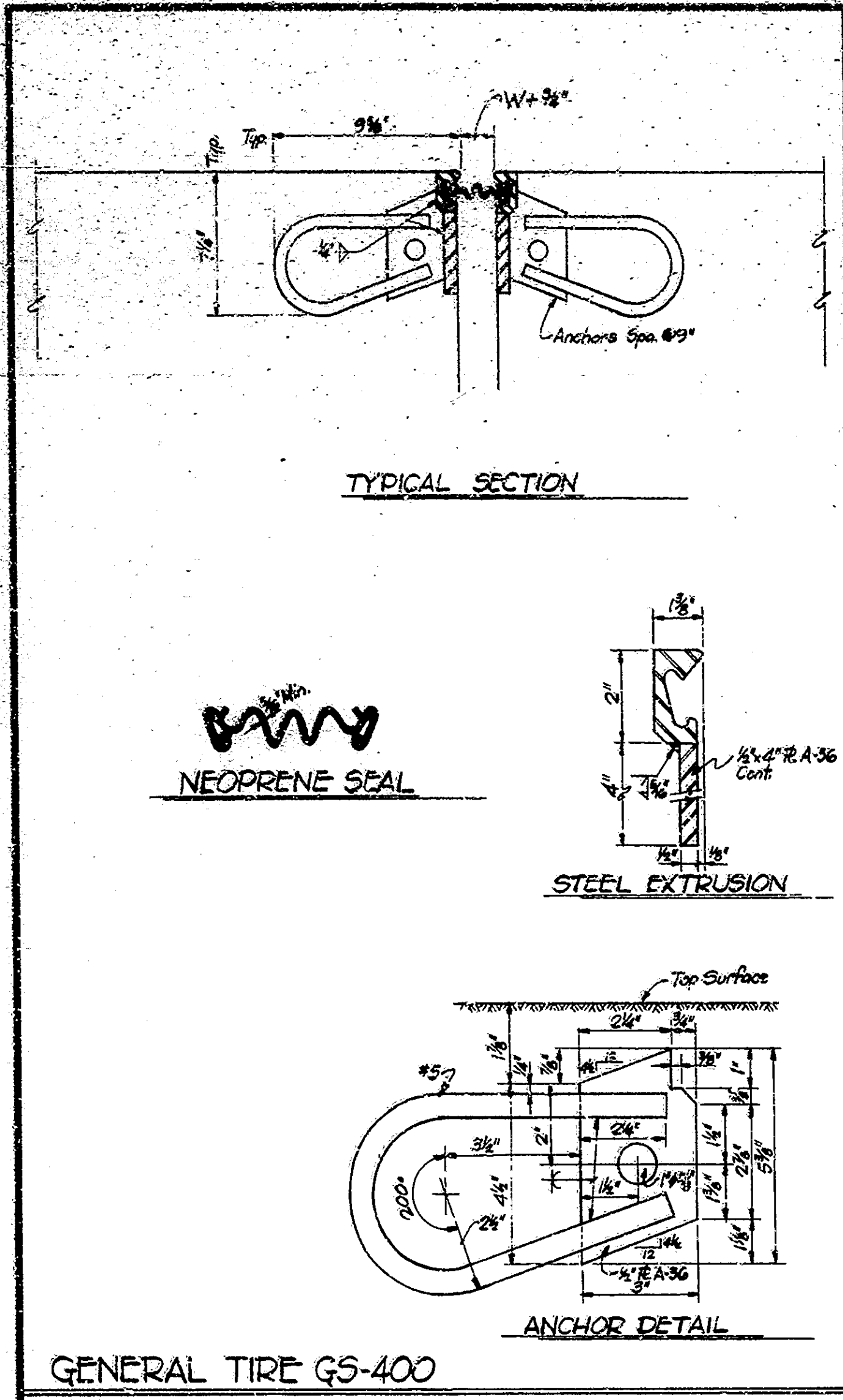
PEDESTAL DETAILS Scale: 1/2" = 1'-0" (2 Req'd)

SUPERSTRUCTURE INDIANA STATE HIGHWAY COMMISSION

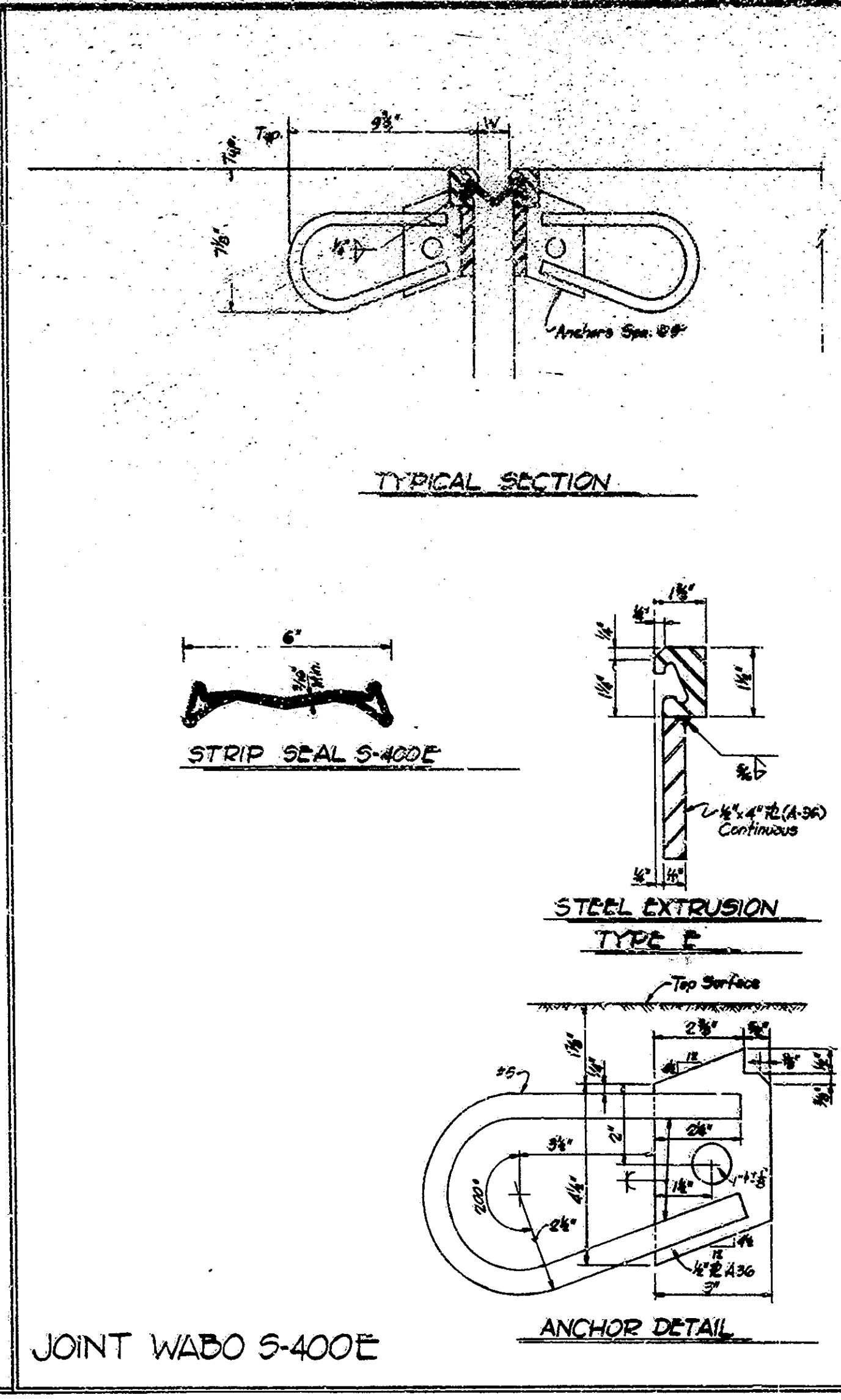
SCALE: As NOTED DATE: May 3, 1979
 SUBMITTED FOR APPROVAL: *Walter M. ...*
 DRAWING: R4 OF R6 SHEET: 6 OF 26
 PROJECT: FR-056-1(2)
 CONTRACT NO. B-13745
 BRIDGE FILE: 154-77-3636-A



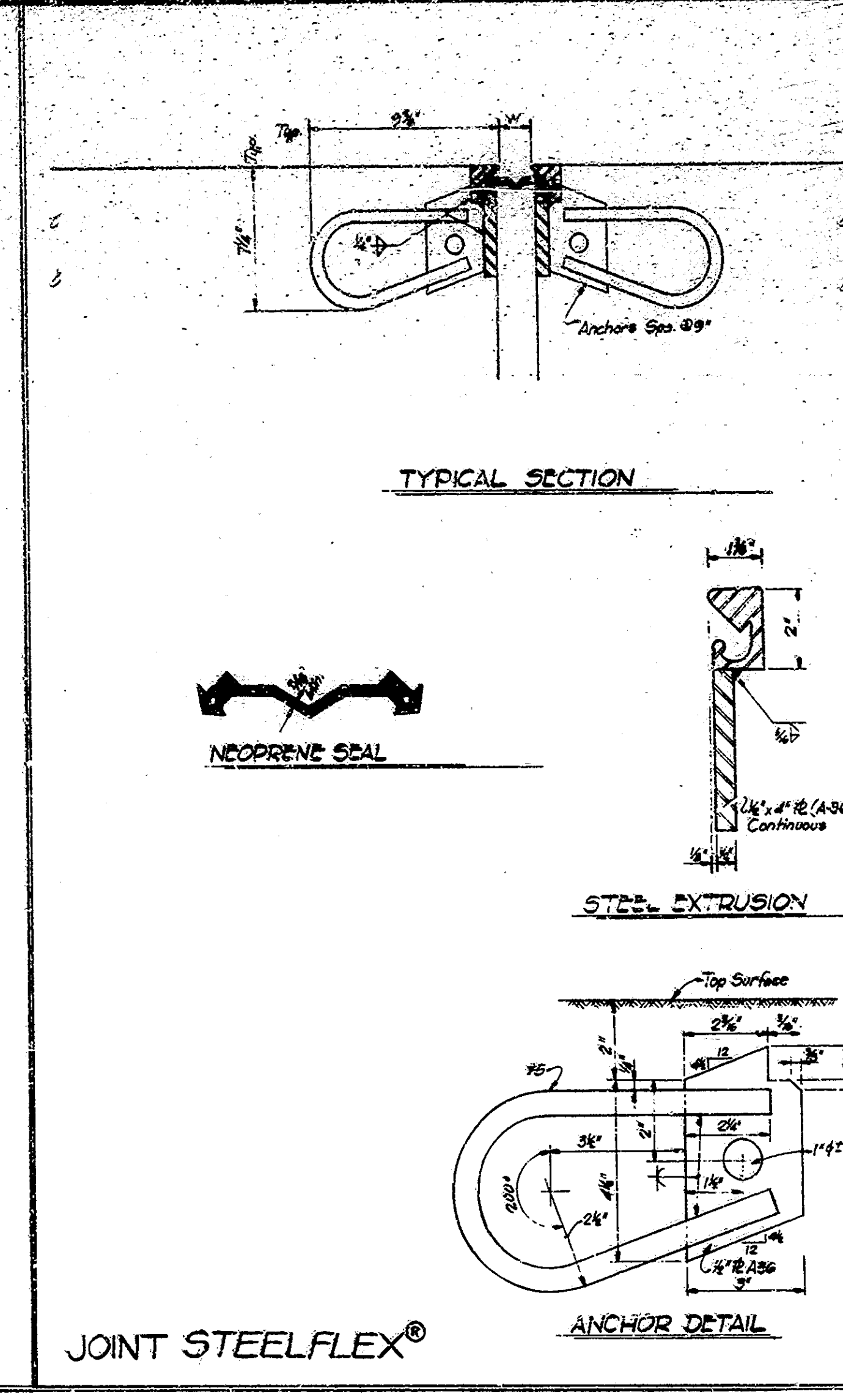
DESIGNED: CKD
 DRAWN: D.W.H.
 TRACED: CKD



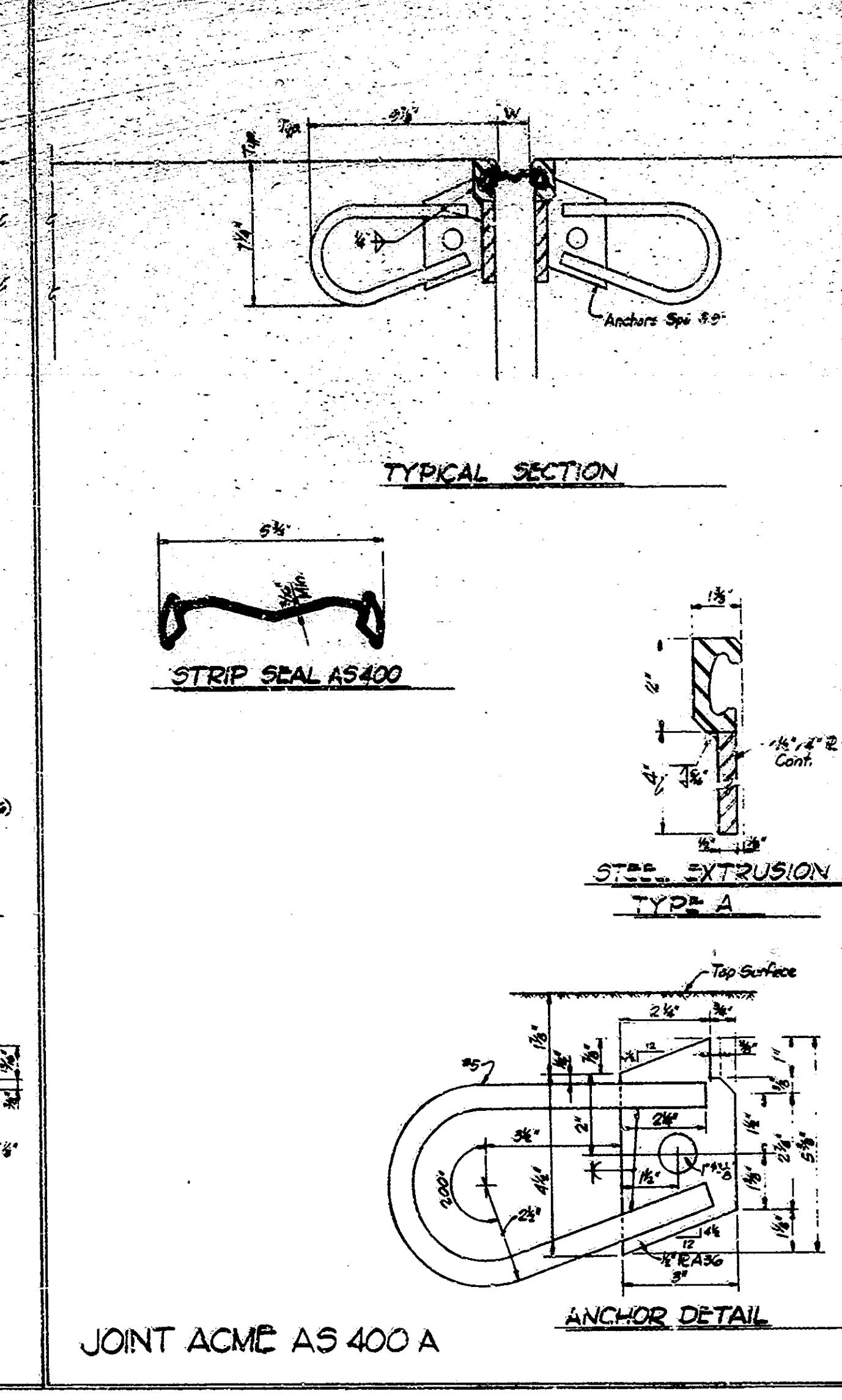
GENERAL TIRE GS-400



JOINT WABO S-400E



JOINT STEELFLEX®



JOINT ACME AS 400 A

NOTES

SEE THE SPECIAL PROVISIONS FOR PROPERTIES OF MATERIALS.
 THE STRIP SEAL GLAND SHALL BE SIZED TO ACCOMMODATE AT LEAST FOUR (4) INCHES OF MOVEMENT.
 THE STRIP SEAL GLAND SHALL BE INSTALLED IN AN EXTRUDED MOUNTING UNIT.
 THE COST OF EXTRUSIONS, ELASTOMERIC SEAL ELEMENTS, SEALANTS, ADHESIVE, CEMENT GROUT, ANCHOR SYSTEM AND INSTALLATION OF JOINT SHALL BE INCLUDED IN THE COST OF EXPANSION JOINT.
 THE PROFILE OF THE JOINT IS TO CONFORM TO THE ROADWAY CROSS SECTION.
 THE SEAL ELEMENT SHALL BE MOULDED AND FURNISHED IN A CONTINUOUS LENGTH EQUAL TO THAT REQUIRED FOR THE JOINT.
 AT CURBS OR IN OTHER LOCATIONS, MEDIAN BARRIERS, ETC. THE SECTIONS OF JOINT ARE TO BE CUT TO THE CURB OR BARRIER TO PROVIDE THE SAME CROSS SECTION ON EACH SIDE BEING JOINED.
 THE ANCHOR SHALL BE TO BE SHOP FABRICATED AND DELIVERED TO THE JOB SITE AS A COMPLETE CONTINUOUS UNIT FOR JOINT LENGTHS UP TO 40 FEET. JOINTS IN THE LENGTHS OF 44 FEET OR JOINTS USED WITH SPACE CONSTRUCTION SHALL BE FIELD WELDED WITH ENDS TO BE SHOP PREPARED.
 ALL WORK, BOTH SHOP & FIELD, SHALL BE IN ACCORDANCE WITH 7103.
 ALL EXPOSED STRUCTURAL STEEL SURFACES WILL BE PRIMED IN ACCORDANCE WITH ISIC STANDARD SPECIFICATIONS.
 THE CONTRACTOR SHALL SUBMIT 3 COPIES OF SHOP DRAWINGS FOR ALL JOINTS INVOLVING CURBS OR OTHER SPECIAL FEATURES.

Ambient Temperature	DIMENSION "W"		
	Expansion Length		
	100'-200'	200'-300'	300'-400'
120°	2 1/4"	2 3/4"	3"
100°	2 3/4"	3"	3 1/4"
80°	3"	3 1/4"	3 3/4"
60°	3 1/4"	3 3/4"	4"
40°	3 3/4"	4"	4 1/4"
20°	4"	4 1/4"	4 3/4"
0°	4 1/4"	4 3/4"	5"

NOTE: EXTRUSIONS WITH HEIGHTS BETWEEN 1 1/2" and 2" MAY BE SUBSTITUTED FOR THOSE SHOWN. IF SUCH A SUBSTITUTION IS MADE, THE NOTCH IN THE ANCHOR PLATE AND THE STRIP SEAL GLAND SHALL BE MODIFIED ACCORDINGLY AND IN CONFORMANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.

EXPANSION JOINTS CLASS S-S
 INDIANA DEPARTMENT OF HIGHWAYS

SCALE: NONE DATE: JULY 15, 1983

James C. K...
 SENIOR DESIGNER

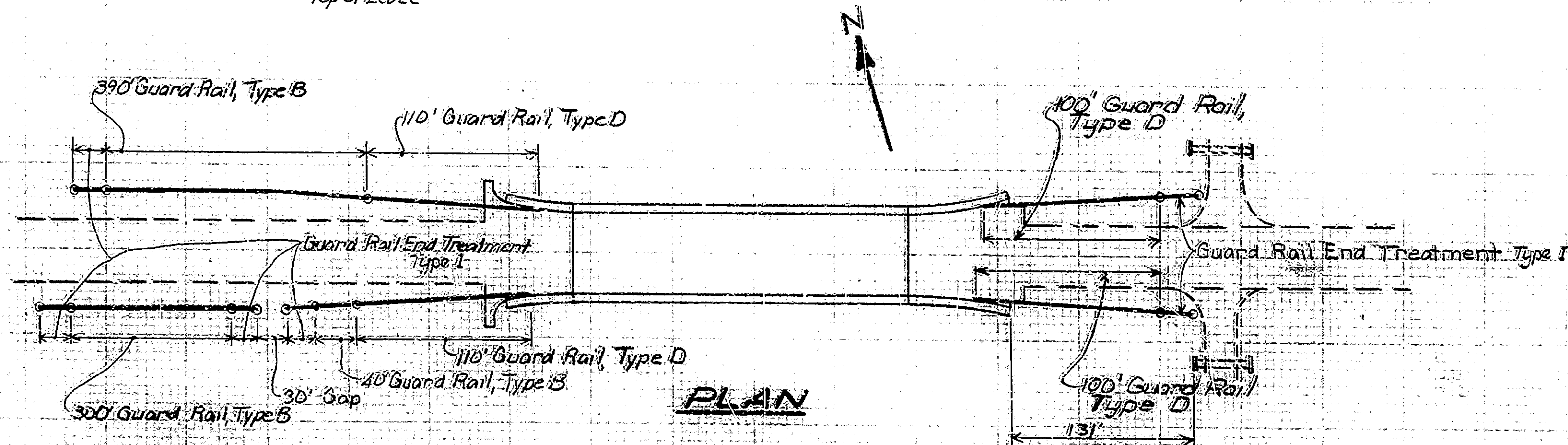
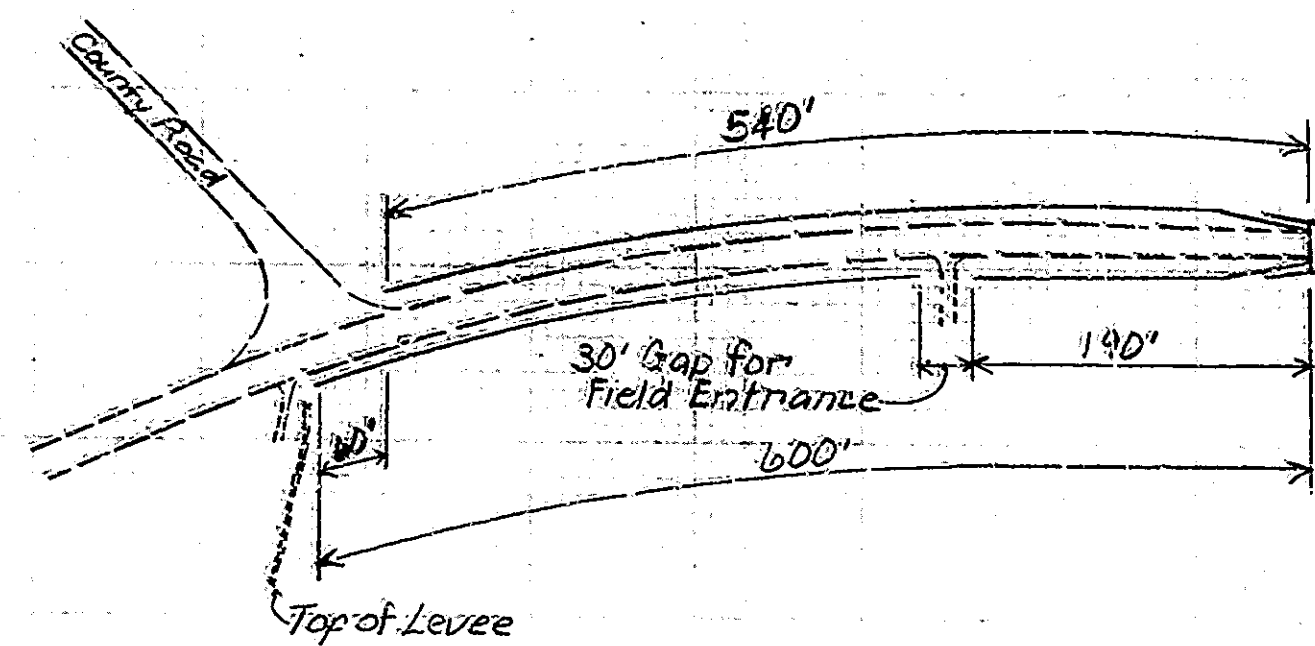
DRAWING: R5A OF R6 SHEET: 7A OF 26
 PROJECT: FR-056-1 (B)
 CONTRACT NO. B-13745
 BRIDGE FILE: 154-77-3636A



DESIGNED: CKD
 DRAWN: CKD
 TRACED: CKD

- A. Includes 147 SY for Scarifying Approach Pavement at end of wedges.
- B. Estimated quantity for patching concrete curbs and walks, includes concrete removal, sawcuts, clearing, reinforcing and epoxy bonding compound.
- C. Includes 300 lbs. of #5 reinforcing as on undistributed quantity to replace badly corroded deck reinforcing.
- D. Includes preparation of curbs and walks.
- E. Includes removal of slab @ Abutments #1 and #2.
- F. Includes:—

Bituminous Wedge and Leveling = 67.4 Tons
 Relief Joints = 9.7 Tons
 Bituminous Widening = 157.6 Tons
236.7 Tons



GUARD RAIL TYPE B*	GUARD RAIL TYPE D*	GUARD RAIL END TREATMENT	GUARD RAIL REMOVAL
NW 390 LF	NE 110 LF	East End 2 EA Type I	NE 42.5 LF
SW 340 LF	SE 100 LF	West End 4 EA	SE 42.5 LF
Total = 730 LF	NW 110 LF	Total = 6 EA	Total = 85 LF
	SW 110 LF		
	Total = 420 LF		

* The cost of removing existing wooden guard rail posts within the limits of Type B or Type D Guard Rail End Treatment shall be included in the cost of Guard Rail, Type B and Type D, respectively.

Rev. 9-13-85 Add Add'l Pr. Deck Overlay
 Rev. 7-15-83 Add Reporting
 Rev. 11-20-82 Summary (Temp. Traffic Signals added)
 Rev. 12-7-82 Summary (Full Depth Patching added)
 Rev. 3-11-83 Summary (Guard Rail Removal, Spec. Type, Surface Seal, Add Conc. Scar del.)

SUMMARY			
CODE NO	DESCRIPTION	UNIT	QUANTITY
51833	Concrete Scarifying	SY	523
	Additional Guide Jack Overlay	CY	4
51837	Bituminous Seal Overlay	SY	391
	Bridge Deck Patching	SF	352
	Bridge Deck Overlay	SY	391
51838	Finishing and Grading	SY	391
	Full Depth Patching	SF	95
51870	Anchor Plates (W-APL)	EA	2
51875	Special Class A Concrete	SF	10
51876	Surface Seal	LSUM	1
	Removal of Bituminous Surface	SY	223
51870	Repairing Masonry Structures	SF	50
51001	Concrete Class A in Superstructure	CY	7.0
51050	Reinforcing Steel	LB	2019
51885	Expansion Joint Type B5/B	LF	36
	Expansion Joint Type S/S	LF	33
51328	Removal Present Structure (Portions)	LS	1
52505	Guard Rail Type B	LF	730
52515	Guard Rail Type D	LF	420
52535	Removal of Guard Rail	LF	85
	Guard Rail End Treatment Type I	EA	6
	Field Drilled Holes in Concrete (for Pedestal)	EA	8
	Drilled Holes for Mudacking	EA	12
	Material for Mudacking	CY	6
	Temporary Traffic Signals	LS	1
52303	Removal of Pavement	SY	10
52470	Bituminous Mixture for Approaches	TON	236.7
	Bituminous Material for Tack Coat	SY	300
52361	Temporary Pavement Marking Type I	LF	408
52640	Maintaining Traffic	LS	1
	Temporary Concrete Barrier	LF	520
52340	Construction Sign Type A	EA	21
52345	Construction Sign Type B	EA	4
	Signal Head Sign 4530-A	EA	4
52362	Temporary Pavement Marking Type II	LF	1850
	Removal of Line Solid White 4"	LF	751
	Removal of Line Solid Yellow 4"	LF	400
	Line Solid White 4"	LF	1122
	Line Solid Yellow 4"	LF	1122

* Estimated quantity for surface seal is 2410 sfl. (See the special Provisions.)

SUMMARY

INDIANA STATE HIGHWAY COMMISSION

SCALE-

DATE- May 3, 1975

SUBMITTED FOR APPROVAL-

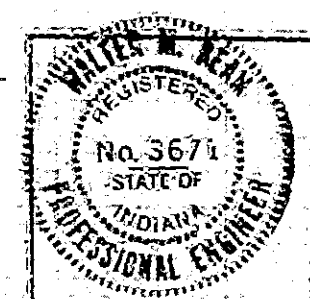
Walter M. Beaman

DRAWING: RG OF P-6 SHEET: 8 OF 26

PROJECT: FR-056-112

CONTRACT NO. B-13745

BRIDGE FILE: 154-77-3636A



END STR